

Exhibit B - Planning Commission Staff Report

- 5.1 Staff Report (For Possible Action): Case No. **LDC25-00003 (2400 West 7th Street)** - A request has been made for: 1) a tentative map for a 28-lot single-family detached subdivision; and 2) a major site plan review for cluster development. The ±3.72 acre project site is located directly south of the intersection at West 7th Street and Rhode Island Drive. The site is located in the Single-Family Residential – 8 units per acre (SF-8) zoning district and has a Master Plan land use designation of Single Family (SF). **[Ward]**

**PLANNING COMMISSION
STAFF REPORT**

Date: November 21, 2024

To: Reno City Planning Commission

Subject: Staff Report (For Possible Action): Case No. LDC25-00003 (2400 West 7th Street): A request has been made for: 1) a tentative map for a 28-lot single-family detached subdivision; and 2) a major site plan review for cluster development. The ±3.72 acre project site is located directly south of the intersection at West 7th Street and Rhode Island Drive. The site is located in the Single-Family Residential – 8 units per acre (SF-8) zoning district and has a Master Plan land use designation of Single Family (SF).

From: Leah Piccotti, Associate Planner

Ward #: 5

Case No.: LDC25-00003 (2400 West 7th Street)

Applicant: AMH

APN: 005-122-09

Request:

1. **Tentative Map:** To allow for a 28-lot single-family detached subdivision.
2. **Major Site Plan Review:** To allow cluster development.

Location: See Case Maps (**Exhibit A**)

Proposed Motion: Based upon compliance with the applicable findings, I move to approve the tentative map and major site plan review, subject to conditions listed in the staff report.

Summary: The ±3.72 acre project site is located within the Single-Family Residential – 8 units per acre (SF-8) zoning district. The proposed tentative map will allow for a 28-unit single-family detached residential subdivision with associated common areas (**Exhibit B**). The SF-8 zoning district requires a minimum lot size of 7,000 square feet for a corner lot and 6,000 square feet for an interior lot. The proposed tentative map utilizes cluster development to allow for an average lot size of ±3,705 square feet. Key issues analyzed include: 1) overall site design; 2) compatibility with surrounding uses and development; 3) traffic, access, and circulation; 4) Master Plan policy guidance; 5) public utility infrastructure; 6) drainage; and 7) tree preservation. With all of the

recommended conditions of approval, the proposed site design meets minimum standards included in the Reno Municipal Code (RMC) and addresses applicable findings. Staff recommends approval, subject to the conditions listed in this staff report.

Background: The site is developed with an ±8,208 square foot building built in 1967, currently used as a religious assembly, and is partially landscaped with 31 mature trees (**Exhibit C**). As proposed, the project will demolish the existing building and remove all 31 mature trees.

Discussion: Staff reviewed the tentative map and major site plan review application and made recommendations consistent with RMC and Reno Master Plan goals and policies during the case review period. Although the applicant has made minor modifications to the plan from the original submittal based on staff recommendations, the majority of design changes suggested/proposed by staff were not implemented. Staff supports the request with the recommended conditions of approval, which generally reflect staff review comments and design recommendations. With the recommended conditions, the project meets minimum RMC standards and addresses the applicable findings.

Per RMC 18.09.04, the definition of Cluster Development is “moderate density attached or detached development, that is designed to protect sensitive areas to allow for common open space.” Per RMC 18.04.903(a)(6), cluster development is encouraged to support the protection of sensitive natural resources, viewsheds, or other unique site features; promote fire safety within the wildland interface; provide opportunities for shared common open space; protect documented wildlife corridors; and provide a more gradual transition to the unincorporated county and public lands. The applicant asserts that the project is “*designed to provide better viewshed protection than non-clustered development through a combination of increased setbacks/buffering and elevation change. Further, the project design proposes improvements to help meet unique site features to address off site drainage constraints. These improvements are provided in the proposed shared common open space which will also include amenities to help make the common open space more usable for the residents*” (**Exhibit D**). Drainage constraints on the western project site prevent lot matching and require lot elevation changes under traditional or clustered subdivision design, which further viewshed protection. Clustered subdivisions typically support variation from nontraditional lot configuration with innovative design solutions, such as landscaped pedestrian-oriented paseos, significant buffering, parks, and enhanced site and building design. With the inclusion of the limited useable open space and the recommended conditions of approval, the request meets the minimum criteria for cluster development.

The applicant asserts that the proposed cluster development better meets the intent of code than other uses allowed in the SF-8 zoning district. While this project does meet the minimum code standards, alternative design approaches could be implemented to accommodate a different site configuration and/or product type (i.e. attached single family, patio homes, zero lot line, “t-courts,”

etc.) The proposed design contemplates a specific detached single family product type envisioned by the applicant. The Master Plan encourages infill development with increased density and enhanced design along neighborhood corridors. The SF-8 district requires a minimum lot size of 6,000 square feet which technically fails to achieve the allowed density of 8 dwelling units per acre. This project does not propose increased density, just a reduction in lot sizes, resulting in an overall density of 7.5 dwelling units per acre. Cluster development would allow for up to a 15% increase in density. For example, the site could be developed with an attached product with up to 34 dwelling units or a combination of attached and clustered detached lots along the western perimeter to further enhance viewsheds. A project design with an attached product, clustered zero lot line, or alternative layout could allow for increased buffering, more useable common open space, and enhanced project design as intended in cluster development and encouraged in the Master Plan. The applicant contends that the proposed detached product/design is more complementary to existing homes within the surrounding area and better maintains neighborhood character.

Analysis:

Overall Site Design and Architecture: The project design consists of 28 single-family detached homes and associated common areas accessed from West 7th Street via a central roadway terminating into a cul-de-sac. The proposed lots range in size from ±3,261 square feet to ±5,000 square feet (**Condition No. 6**). A riprapped landscape buffer/common open space is incorporated along the project perimeter. Landscaped common open space, consisting of ±8,000 square feet, will be located in two triangular areas south of Lot 10 and west of Lot 11 and is proposed to include pedestrian benches and an ADA accessible route connecting the two areas (**Condition No. 7**). Access to the useable common areas is through a private alley with no proposed sidewalk. All common areas will be fully landscaped and maintained by the Homeowners Association (HOA) or equivalent entity (**Condition No. 8**).

The proposed homes are two story, with the square footage ranging from ±1,851 to ±2,395 square feet, and are a maximum of 28-feet in height (**Condition No. 9**) (**Exhibit E**). As designed, all homes include a two-car garage and a two-car driveway, providing four parking spaces per home for a total of 112 spaces (**Condition No. 10**) plus on-street parking.

Development standards for the SF-8 zoning district compared with the proposed cluster development are shown in the table below.

Lot Standard Comparison		
	<u>SF-8</u>	<u>Proposed</u>
Max. Base Density	8 dwelling units per acre ¹	7.5 dwelling units per acre
Minimum Lot Size	7,000 square feet corner lot 6,000 square feet interior lot	3,261 square feet
Lot Standard Comparison		
	<u>SF-8</u>	<u>Proposed</u>
Minimum Lot Width	70 feet corner lot 60 feet interior lot	35 feet
Front/Side/Rear Setbacks	10/5/20 feet	10/5/20 feet
Street Facing Garage	20 feet	20 feet
Building Lot Coverage	40%	40%
Maximum Stories/Height	2.5 stories/35 feet	2 stories/28 feet

1 - Note: minimum lot sizes restrict maximum density to 7.26 dwelling units per acre.

Landscaping: As proposed, the residences along West 7th Street will be ±10 feet from the existing property line with the side of the two-story homes facing the West 7th Street frontage. Infill development standards require that minimum setbacks for new developments match the existing setback of surrounding properties, which is requested to be modified through cluster development. The properties to the east and west along the south side of West 7th Street are setback approximately 15 feet from the property line. The homes on the north side of West 7th Street, are setback ±20 feet. There is a span of ±20 feet from back of curb from West 7th Street to the proposed lot line of the two street fronting parcels. **Condition No. 11** is recommended to ensure that a parkway strip with enhanced landscaping starting from back of curb and a new ADA compliant five foot sidewalk are incorporated to soften the visual impact. The applicant has also requested that Planning Commission waive RMC standards requiring a minimum five-foot-wide landscaped parkway on the internal street to accommodate the clustered lot layout. This allows for further separation (five additional feet) between new homes and existing residences to the west.

Staff requested that the applicant submit a landscape plan identifying all existing trees. The intent of this was to determine which trees will be impacted and propose appropriate mitigation to avoid impacts and/or preserve existing healthy mature trees. There are Jeffrey and/or Ponderosa Pines onsite. While RMC allows for mature trees to be removed if it is determined necessary for proposed improvements, Master Plan policies SD.4, GP 7.7A, and 7.7B provide guidance for the preservation of mature trees during the discretionary review process. As proposed, grading along the western perimeter calls for the removal of the existing pine trees. Per the applicant, mature pine trees along the western boundary are located under an existing powerline and it is NV

Energy’s stance that these trees should be removed to protect the powerline. Staff recommends that further analysis be completed including consultation with a licensed arborist to determine if the trees could be preserved with the proposed grading design, or minor alterations thereto, as well as consultation with NV Energy to determine if any identified trees to be saved can be trimmed in accordance with NV Energy guidelines and preserved (**Exhibit F**). Evidence of this shall be submitted to the satisfaction of the Administrator with the site improvement permit and any identified viable pine trees shall be preserved (**Condition No. 12**).

Site Compatibility and Adjacency Standards: Land Use Compatibility: Land uses surrounding the site consist of single-family residential uses to the north and west, and Mountain View Cemetery to the east and south and are summarized in the table below.

Adjacent Properties		
	Zoning	Use
North	SF-8	Single-Family residences
East	No Zoning	Highland Ditch (TMWA) & Mountain View Cemetery
South	No Zoning	Highland Ditch (TMWA) & Mountain View Cemetery
West	SF-8	Single-Family residences

Along the west side of the property, the applicant has proposed a common area buffer ranging from 5 to 22 feet. With the 20 foot rear yard setback, this will result in an overall separation between the shared property lines and the proposed homes ranging from 28 feet to 43 feet, as shown in **Exhibit B**. The applicant contends that, in combination with the cluster design proposed, the western buffer will increase separation between new and existing homes over traditional development design and set finished floor elevations of new homes at a lower elevation than the existing homes to the west, thereby protecting the neighbor’s viewsheds.

RMC 18.04.903(a)(7)(d) states “*to provide adequate transition between varying sizes of single-family residential parcels designated for greater than one unit per acre density, one of the following methods shall be utilized: parcel size matching, buffering, yard matching, height matching.*” RMC offers four methods: parcel size matching, a 30-foot-wide landscaped buffer, yard matching, or height matching. Staff requested that the applicant consider utilizing one of these four methods. However, this section of code is only compulsory with *new land divisions adjacent to lower density residential zoning districts*. Since the adjacent zoning district is also SF-8, this section does not technically apply. The intent of this section is to *provide adequate transition between varying sizes of single-family residential parcels*. These methods of transition are commonly utilized on similar projects. The proposed plan includes a buffer along the western perimeter ranging from 16 feet to 22 feet (adjacent to Lots 1 through 10) and a 5-foot buffer at Lot 11. This buffer, combined with proposed building setbacks, results in new homes being separated from the shared western property line at an average of ±34 feet. The applicant contends that this is consistent with the intent of RMC 18.04.903(a)(7)(d). While the design meets RMC requirements, an increase to the buffer width would further increase compatibility with the existing neighborhood

to the west and the amount of provided common/open space, further supporting viewshed protection and the intent of cluster development criteria.

The western buffer shall be landscaped per RMC standards. **Condition No. 13** requires that landscape plans be approved demonstrating that all rear yards are fully landscaped. Lots one through ten, along the western boundary, shall provide a minimum of three evergreen trees in addition to the required landscaping for all rear yards. Installation shall be completed, or appropriate security provided, prior to issuance of the first certificate of occupancy. Maintenance shall be the responsibility of the homeowners and shall be enforced through appropriate CC&R's outlined in the HOA or equivalent entity.

Per RMC 18.04.903(c)(1), additional setbacks and stepbacks are required for compatibility in the front and side yards. A 2:1 building height/setback ratio shall be maintained from all front setbacks, including those abutting West 7th Street, which has two fronts. In the side yard, any portion of the building above 24 feet in height shall have a minimum of an additional one-foot horizontal side yard setback or 1:1 building height/side stepback or combination thereof, as shown in **Exhibit G (Condition No. 14)**.

Per RMC 18.04.903(c)(3), single-family residences 2,000 square feet or greater, shall incorporate a minimum of four design elements along all street-facing façades and a minimum of two design elements on all other façades. Single family residences less than 2,000 square feet shall incorporate a minimum of three design elements along all street-facing façades and one design elements on all other facades. Since the square footage will vary, all homes shall incorporate a minimum of four design elements along all street-facing façades and two design elements on all other façades (**Condition No. 15**). Additionally, to increase compatibility with the adjacent properties, the rear façade of all residences along the west property line shall incorporate a minimum of three design elements (**Condition No. 16**).

Per RMC 18.04.903(c)(4), *“ground floors of street facing facades shall have transparent doors or windows no less than three feet in height along at least 20 percent of the façade’s horizontal length.* Per RMC 18.04.904(a)(2)(b), *new single-family residential structures on lots narrower than 70 feet shall utilize a minimum of two techniques to reduce the prominence of garages, promote pedestrian activity, and create visual diversity in single-family neighborhoods.”* As shown in **Exhibit E**, the proposed residences are garage forward products. As designed, they do not meet the required Ground Floor Interest or Standard Street Images Features. In response to this, the applicant stated that they will provide two of the following: varied setbacks, enhanced landscaping, front door path, garage/front door windows, or structure articulation (**Condition No. 17**).

Lastly, to mitigate impacts on the surrounding properties the hours and days of construction shall be limited (**Condition No. 18**).

Public Improvements: This is an infill project and all necessary utilities to serve the project are already in place and/or can be extended to the site. The project will be served by the Truckee Meadows Water Authority (TMWA) for water, Waste Management (WM) for trash, City of Reno for sewage, NV Energy for gas and electricity, AT&T for telephone and Charter Communications for cable. Water and sewer infrastructure will be replaced and upgraded to serve the additional increase in demand. All new infrastructure required to serve the project (i.e. sewer, water, drainage) will be further analyzed at the final map permit processes.

Hydrology: The site is located within an Unshaded Zone X flood designation area, which indicates that the site is outside the 0.2 percent (500-year) flood plain. The project proposes onsite retention as a method to mitigate the increase of stormwater flows as the result of the project. Storm water retention requires minimum infiltration rates of one inch per hour. If due to low infiltration rates, the site is not suitable for retention, runoff from the site should be mitigated through detention and discharged to West 7th Street. No discharge will be permitted to the highland ditch. Additional analysis may be required to determine any impacts to downstream facilities. It may be necessary to route flows to existing downstream storm drain infrastructure or provide new storm drain facilities as necessary and determined by the City of Reno.

Traffic, Access, and Circulation: West 7th Street is classified as a minor arterial and can handle the additional traffic without affecting the Level of Service (LOS). The project is expected to generate 264 average daily trips (ADT) with 20 trips occurring during the AM Peak Hour (PH) and 26 trips occurring in the PM PH. The ADT for the existing church is 62 trips with 6 trips occurring during the AM PH and 8 trips occurring in the PM PH. The net increase in traffic is calculated as 202 ADT with 14 AM PH trips and 18 PM PH trips. The project will have a minimal impact on the West 7th Street and Rhode Island Drive intersection. To alleviate any impact to the intersection, the project will be required to install a left turn pocket along West 7th Street to access the development (**Condition 19**). All improvements shall be completed in conformance with the Public Works Design Manual and the American Association of State Highway and Transportation Officials (AASHTO).

Access to the site will be provided from West 7th Street. The proposed access meets the City of Reno and RTC access management standards. The traffic circulation, emergency access, and pedestrian access are adequate for the project. The plans are in conformance with the Public Works Design Manual (PWDM) and Article 6 (Access, Connectivity, and Circulation), Section 18.04.601 of the RMC.

The Regional Transportation Commission (RTC) has identified a Bus Line 4 stop on West 7th Street adjacent to the project. Prior to the approval of the final map, the applicant shall provide

documentation that RTC has reviewed and approved the plans to temporarily relocate the bus stop during construction. Once the project has been completed, the current bus stop location shall be maintained and improved to the satisfaction of RTC and comply with ADA standards (**Condition 20**).

Public Services: No noted concerns were received from either the Reno Fire Department or Reno Police Department regarding this request. The closest fire station is Station 8 located at 3600 Kings Row. The current response time from Station 8 is four minutes.

Master Plan Conformance: The subject site has a Master Plan land use designation of Single-Family Neighborhood (SF), is located in a Central Neighborhood, and abutting a Neighborhood Corridor per the Structure Plan Framework of the Reno Master Plan. As outlined in the Master Plan, infill development within the Central Neighborhoods should utilize transition techniques such as increased setbacks to incorporate landscape buffers, and front setbacks should be within the range of other front setbacks along the block. Additionally, infill development should be designed to fit with surrounding buildings, incorporating similar heights, lot coverages, and widths in its design. Neighborhood Corridor policies support building orientation towards the corridor with a higher level of design and pedestrian access. With the recommended conditions of approval, the project is in general conformance with the applicable Master Plan land use designation and the following applicable Master Plan goals and policies:

- Citywide Policy 2.2B: Underutilized Properties
- Citywide Policy 4.3B: Infill and Redevelopment
- Area Specific Policy C-NC.4: Tree Canopy
- Area Specific Policy C-NC.5: Building Orientation
- Area Specific Policy C-NC.6: Housing Options
- Area Specific Policy N-CN.8: Transitions
- Area Specific Policy N-CN.6: Building Orientation and Setbacks

Public and Stakeholder Engagement: The project was reviewed by various City divisions and partner agencies (**Exhibit H**). Courtesy notices and hearing notices were sent out to surrounding property owners. The applicant attended the Ward 5 Neighborhood Advisory Board (NAB) meeting on August 13, 2024. Six comment forms were filled out in opposition to the project at the NAB meeting and staff has received 13 letters of opposition from seven people (**Exhibit I**). Concerns were raised regarding the compatibility of this development with the established neighborhood character. Respondents contend that this request does not meet the definition or intent of cluster development since no sensitive areas are being preserved and the proposed open space is unusable. There are concerns that this section of West 7th Street has a “blind bend” that makes it dangerous for traffic and pedestrians. Additional concerns include the existing bus stop, lighting along West 7th Street, school capacity, and TMWA access to the Highland Ditch. In

addition to the NAB meeting, the applicant also held a community meeting on September 10, 2024. Comments made during the meeting mirror those from the NAB and the public comments received by staff (**Exhibit J**). Future comments will be forwarded to the Planning Commission as they are received.

After receiving public comments, staff contacted Reno Police Department (RPD), TMWA, and Washoe County School District (WCSD). RPD provided an accident report that shows that there have been two accidents at the intersection at West 7th Street and Everett Drive in the past two years. TMWA states that they do not use the property for maintenance of the Highland Ditch, nor will development on the subject property hinder future maintenance. WCSD states that they do not anticipate any issues accommodating students potentially generated by this development.

Recommended Conditions of Approval: All conditions shall be met to the satisfaction of Development Services Department staff, unless otherwise noted.

1. The project shall comply with all applicable City codes, plans, reports, materials, etc., as submitted. In the event of a conflict between said plans, reports, materials and City codes, City codes in effect at the time the application is submitted, shall prevail.
2. The applicant shall record the final map(s) in accordance with the time limit contained in state law or this approval shall be null and void.
3. Prior to the issuance of any building permit or final map, the applicant shall attach a copy of the final approval letter. The approval letter shall accompany a narrative that describes how the requested permit addresses each of the approved conditions of approval.
4. Prior to the approval of the final map, the applicant shall provide an affidavit stating that the subdivider will make provision for payment of the tax imposed by Chapter 375 of Nevada Revised Statutes (NRS) and for compliance with the disclosure and recording requirements of NRS 598.0923, if applicable, by the subdivider or any successor in interest.
5. The applicant, developer, builder, property owner, or business proprietor, as applicable, shall continuously maintain a copy of this approval letter on the project site during the construction and operation of the project/business. The project approval letter shall be posted or made readily available upon demand by City staff.
6. The final map shall demonstrate that minimum lot size is $\pm 3,261$ square feet.
7. Prior to the issuance of a building permit for vertical construction, landscape plans shall be approved demonstrating that all common areas shall be landscaped, per RMC standards and an ADA compliant pedestrian access connecting the two common areas (west of Lot 11 and south

of Lot 10). A minimum of two pedestrian benches shall be placed in the common open space, as shown in **Exhibit D**. Installation shall be completed prior to issuance of the first certificate of occupancy.

8. Prior to the approval of a final map, the applicant shall provide suitable documentation that a homeowner's association or equivalent entity will be formed to provide maintenance of all project common areas, and have said documentation recorded concurrently with the final map. Notes shall be added to the final map identifying that maintenance responsibility of all common areas shall be the responsibility of the HOA or equivalent entity.
9. All structures shall be limited to a maximum height of 28 feet.
10. Site improvement plans/applicable building permits shall demonstrate a minimum of four (4) off-street parking spaces per unit. This can be a combination of garage spaces, off-street parking stalls, or private driveways.
11. Site improvement and landscaping plans shall demonstrate a minimum five-foot-wide landscaped parkway starting from back of curb along West 7th Street and a minimum five foot-wide sidewalk. The remaining area between the sidewalk and lot parcel line shall be landscaped to the satisfaction of the Administrator. Notes shall be added to the final map identifying that maintenance responsibility of landscape and sidewalk improvements in the right-of-way shall be the responsibility of the HOA or equivalent entity.
12. Prior to issuing any permit, the applicant shall consult with a licensed arborist to determine if any existing pine trees within the western buffer can be preserved under the approved grading scenario or with minor changes thereto. If trees are identified for preservation, NV Energy shall be consulted to determine if said trees can be trimmed and preserved in accordance with the NV Energy guidelines (**Exhibit F**). Evidence of this shall be submitted to the satisfaction of the Administrator with the initial permit application for site improvements. Viable pine trees identified shall be preserved within the western buffer area.
13. Prior to issuing a building permit for vertical construction, landscape plans shall be approved demonstrating that all rear yards be fully landscaped with a minimum of one tree and six shrubs per 300 square feet. Lots one through ten shall provide a minimum of three evergreen trees and 12 shrubs for all rear lots along the western boundary. Installation shall be completed, or appropriate security provided, prior to issuance of the first certificate of occupancy. Maintenance shall be the responsibility of the homeowners and shall be enforced through appropriate CC&R's outlined in the HOA or equivalent entity.
14. Prior to issuing any building permits for vertical development, all homes shall demonstrate a 2:1 building height/setback ratio from all front setbacks, including corner lots abutting

West 7th Street, which have two fronts. In the side yard, any portion of the building above 24 feet in height shall have a minimum of an additional one-foot horizontal side yard setback or 1:1 building height/side step back or combination thereof, as shown in **Exhibit G**.

15. Prior to issuing any building permits for vertical development, all homes shall incorporate a minimum of four design elements, referenced in RMC 18.04.903(c)(3), along all street facing façades and two design elements on all other façades.
16. Prior to issuing any building permits for vertical development, the rear façade of all residences along the west property line shall incorporate a minimum of three design elements referenced in RMC 18.04.903(c)(3).
17. Prior to issuing any building permits for vertical development, the ground floors of street facing facades shall have transparent doors or windows no less than three feet in height along at least 20 percent of the façade's horizontal length and shall utilize a minimum of two of the following techniques: varied setbacks, enhanced landscaping, front door path, or structure articulation.
18. Hours of construction, including grading, shall be limited to between the hours of 7:00 a.m. and 6:00 p.m., Monday through Friday, and between 8:00 a.m. and 6:00 p.m. on Saturday. There shall be no construction on Sundays. This condition shall not apply to dust control or storm water management operations. A note to this effect shall be placed on the title sheet of all building permit plan sets. If the construction hours need to be varied for the pouring of concrete slabs, interior construction hours or other modifications, a plan detailing the construction operations and provisions to minimize impacts on nearby residential areas shall be submitted and approved to the satisfaction of Administrator.
19. Prior to the approval of the final map, the project will be required to install a left turn pocket along West 7th Street to access the development and be completed in conformance with the Public Works Design Manual and the American Association of State Highway and Transportation Officials (AASHTO).
20. Prior to the approval of the final map, the applicant shall provide documentation that RTC has reviewed and approved the plans to temporarily relocate the bus stop during construction. Once the project has been completed, the current bus stop location shall be maintained and improved to the satisfaction of RTC and comply with ADA standards.

Findings:

General Review Criteria: The decision-making body shall review all development applications

for compliance with the applicable general review criteria stated below.

- 1) Consistency with the Reno Master Plan: The proposed development shall be consistent with the Reno Master Plan. The decision-making authority:
 - a. Shall weigh competing plan goals, policies, and strategies; and
 - b. May approve an application that provides a public benefit even if the development is contrary to some of the goals, policies, or strategies in the Reno Master Plan.
- 2) Compliance with Title 18: The proposed development shall comply with all applicable standards in this Title, unless the standard is lawfully modified or varied. Compliance with these standards is applied at the level of detail required for the subject submittal.
- 3) Mitigates Traffic Impacts: The project mitigates traffic impacts based on applicable standards of the City of Reno and the Regional Transportation Commission.
- 4) Provides Safe Environment: The project provides a safe environment for pedestrians and people on bicycles.
- 5) Rational Phasing Plan. If the application involves phases, each phase of the proposed development contains all of the required streets, utilities, landscaping, open space, and other improvements that are required to serve or otherwise accompany the completed phases of the project, and shall not depend on subsequent phases for those improvements.

Tentative Map: Approval of tentative maps shall be subject to the approval criteria in Section 18.08.304(e), Approval Criteria Applicable to All Applications, and criteria set forth in NRS Section 278.349(3), as follows:

- 1) Environmental and health laws and regulations concerning water and air pollution, solid waste disposal, water supply facilities, community or public sewage disposal and, where applicable, individual systems for sewage disposal;
- 2) Availability of water which meets applicable health standards and is sufficient for the reasonably foreseeable needs of the subdivision;
- 3) Availability and accessibility of utilities;
- 4) Availability and accessibility of public services such as schools, police and fire protection transportation, recreation and parks;
- 5) Conformity with the zoning ordinances, master plan, and elements thereof, except that if any existing zoning ordinance is inconsistent with the master plan, the zoning ordinance takes precedence;
- 6) General conformity with the governing body's master plan of streets and highways;
- 7) Effect of the proposed subdivision on existing public streets and the need for new streets or highways to serve the subdivision;
- 8) Physical land characteristics such as flood plain, slope, soil;
- 9) Recommendations and comments of those entities reviewing the tentative map pursuant to NRS 278.330 and 278.348;
- 10) Availability and accessibility of fire protection, including, but not limited to, the availability and accessibility of water and services for the prevention and containment of fires, including fires in wild lands;

- 11) The potential impacts to wildlife and wildlife habitat; and
- 12) Submission by the subdivider of an affidavit stating that the subdivider will make provision for payment of the tax imposed by Chapter 375 of NRS and for compliance with the disclosure and recording requirements of subsection 5 of NRS 598.0923, if applicable, by the subdivider or any successor in interest.

Major Site Plan Review: In addition to meeting the criteria in Section 18.08.304(e), *Approval Criteria Applicable to all Applications*, the following findings shall be made prior to granting a major site plan review permit:

- a. The proposed design is compatible with surrounding development;
- b. The proposed design is consistent with applicable development standards;
- c. Public services and facilities are available to serve the project, or will be provided with development;
- d. The characteristics of the project as proposed and as may be conditioned are reasonably compatible with the types of development permitted in the surrounding area; and
- e. The approval will not be materially detrimental to the public health, safety, or welfare. The factors to be considered in evaluating this application shall include:
 1. Property damage or nuisance resulting from noise, smoke, odor, dust, vibration, or illumination; and
 2. Any hazard to persons and property.

Cluster Development: Minimum lot size may be reduced through clustering of development if, in addition to the general major site plan review findings, the following findings shall be made prior to approving a major site plan review for cluster development:

- a. The clustering proposal, compared with a more traditional site development plan, better attains the policies and objectives of this article, such as providing more open space, preserving existing trees and vegetation coverage, preserving view corridors, and preserving sensitive environmental areas such as stream corridors, slide areas, wetlands, and steep slopes;
- b. The clustering proposal will have no significant adverse impact on adjacent properties or development, or the applicant has agreed to adopt appropriate mitigation measures such as edge matching, landscaping, screening, illumination standards, and other design features to buffer and protect adjacent properties from the proposed clustered development; and
- c. The clustering proposal meets all other applicable requirements set forth in this article or in other applicable ordinances or regulations.

Attachments:

Exhibit A – Case Maps
Exhibit B - Tentative Map
Exhibit C – Photos

Exhibit D – Justification

Exhibit E – Renderings & Floorplans

Exhibit F - NV Energy Tree Trimming Explained

Exhibit G – Front & Side Setbacks & Stepbacks

Exhibit H – RTC Comments

Exhibit I – NAB Public Comment Forms & Public Comment

Exhibit J – Community Meetings

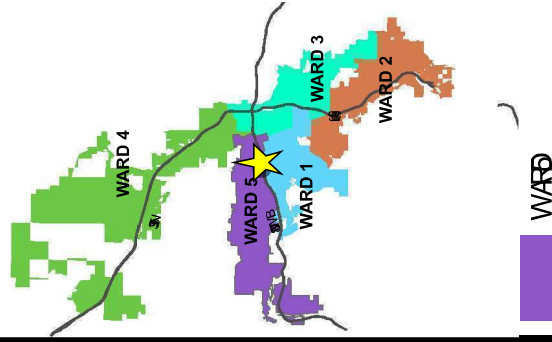
AREA MAP



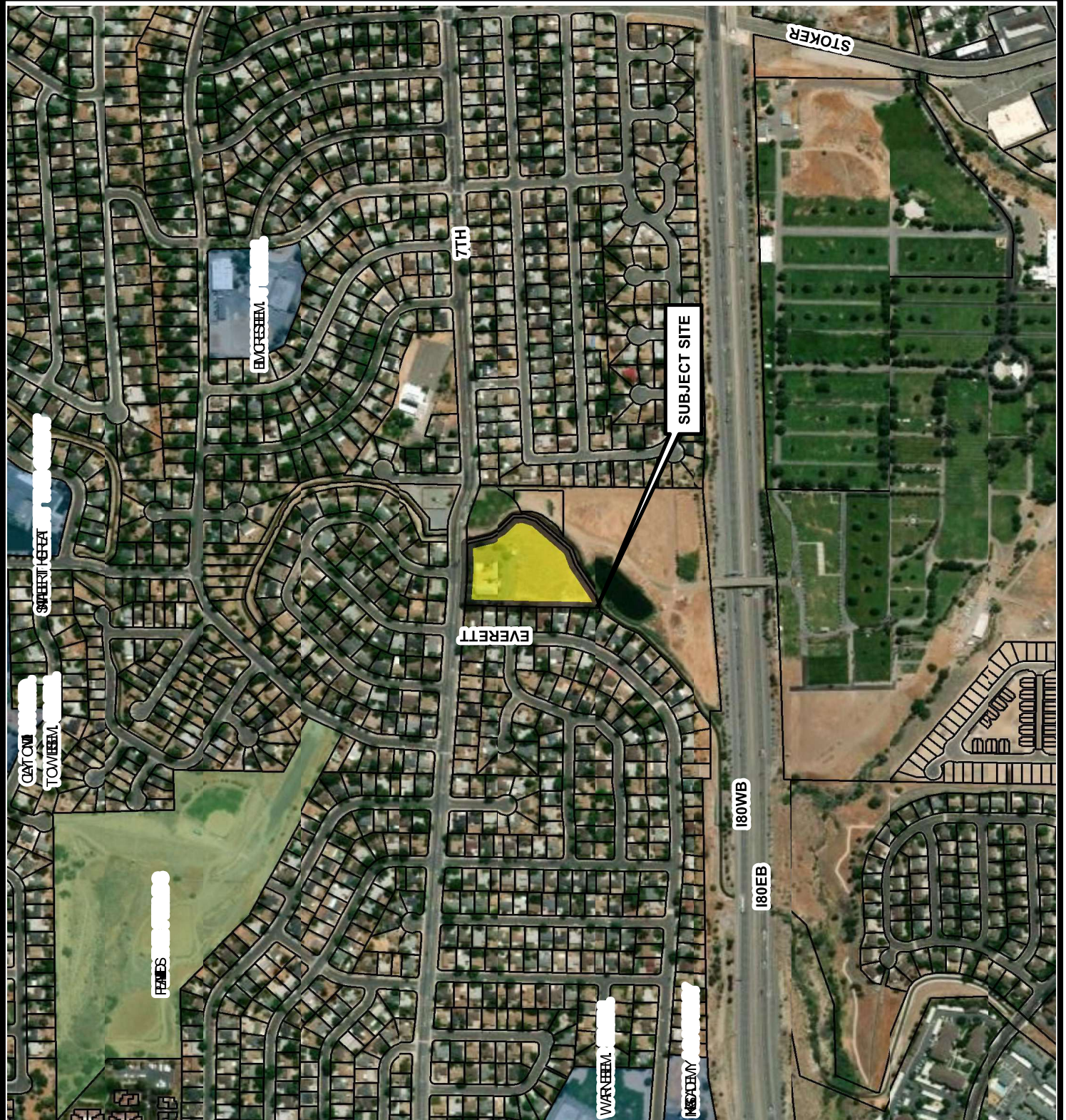
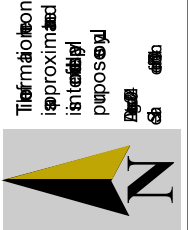
(2400 West 7th Street)



Site




WARD

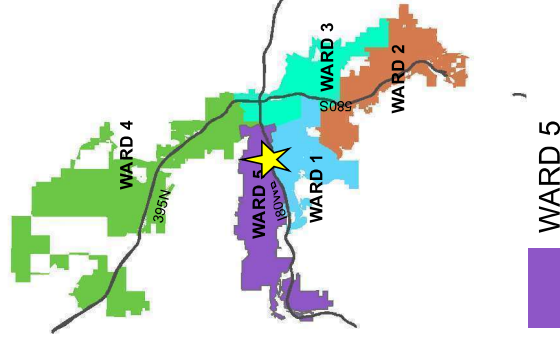


VICINITY MAP

LDC25-00003

(2400 West 7th Street)

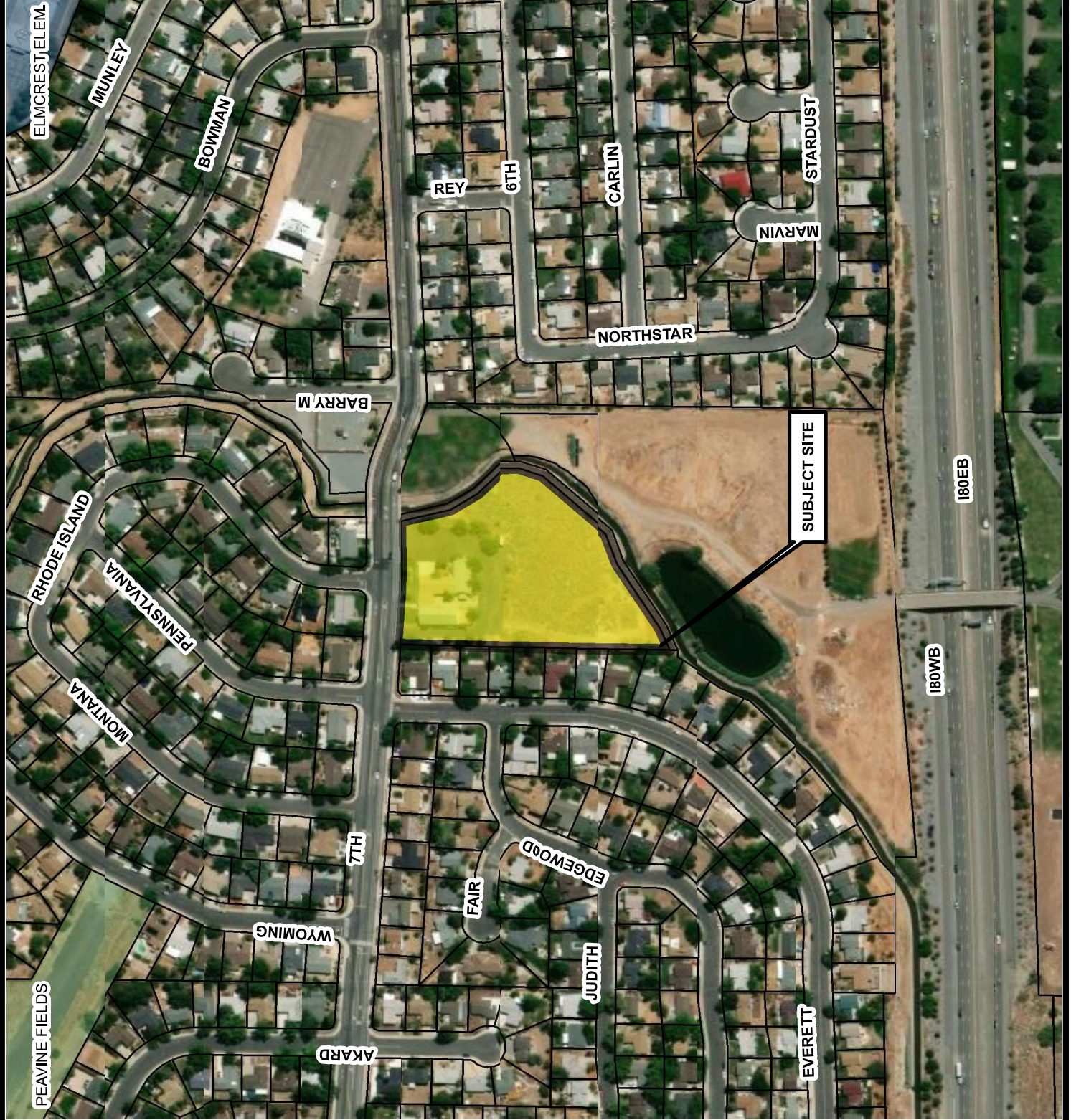
Subject Site ► 



The information herein is approximate and is intended for display purposes only.

Date: August 2024

Scale: 1 inch = 300 feet



ZONING MAP






LDC25-00003

(2400 West 7th Street)

ZONING = SF-8

Subject Site ► 

Zoning Designations

	SF-8
	SF-11
	MF-14
	LLR-1
	PF



Development
Services
Department

The information herein
is approximate and
is intended for display
purposes only.


DATE: August 2024
SCALE: 1 inch = 300 feet

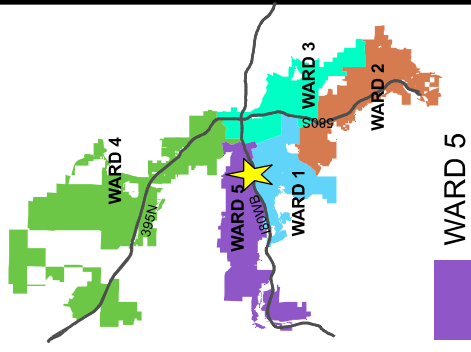


MASTER PLAN MAP

LDC25-00003

(2400 West 7th Street)

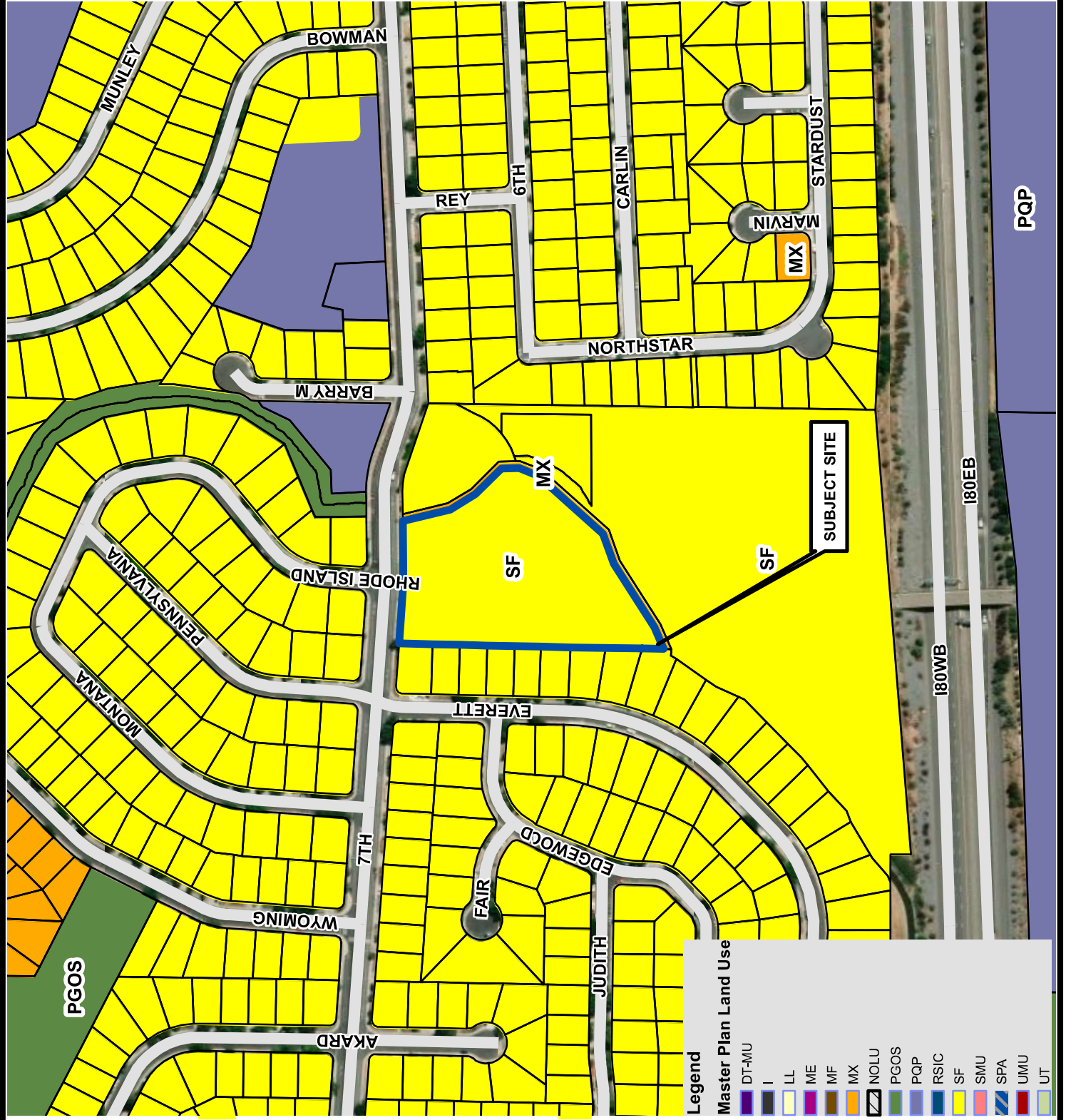
Subject Site 



Development
Services
Department

The information herein
is approximate and
is intended for display
purposes only.

Date: August 2024
Scale: 1 inch = 300 feet



Legend

Master Plan Land Use

DT-MU	I	LL	ME	MF	MX	NOLU	PGOS	PGP	RSIC	SF	SMU	SPA	UMU	UT
-------	---	----	----	----	----	------	------	-----	------	----	-----	-----	-----	----

2400 W. 7TH STREET TENTATIVE MAP TITLE SHEET

OWNER
EPISCOPAL DIOCESE OF NEVADA
PO BOX 2857
RENO, NV 89505

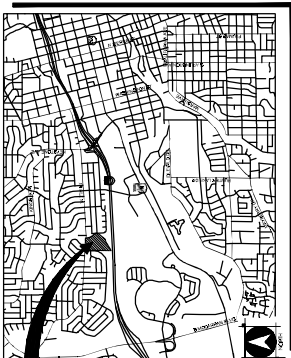
DEVELOPER:
AAH
9585 PROTOTYPE COURT, SUITE B
RENO, NV 89521
775-815-1060

BASIS OF ELEVATION

NEVADA STATE PLANS COORDINATE ELEVATION WITH THE NORTH AMERICAN DATUM (NAD 83) AND ARE BASED ON THE 1988 REAL TIME KAMRAC GPS NETWORK (NAD 83). THE SURFACE ELEVATION DIFFERENCE BETWEEN THE 1988 DATUM AND THE 1988 DATUM IS 0.000000 FEET. THE COMBINED GROUND FACTOR IS 0.000000 FEET.

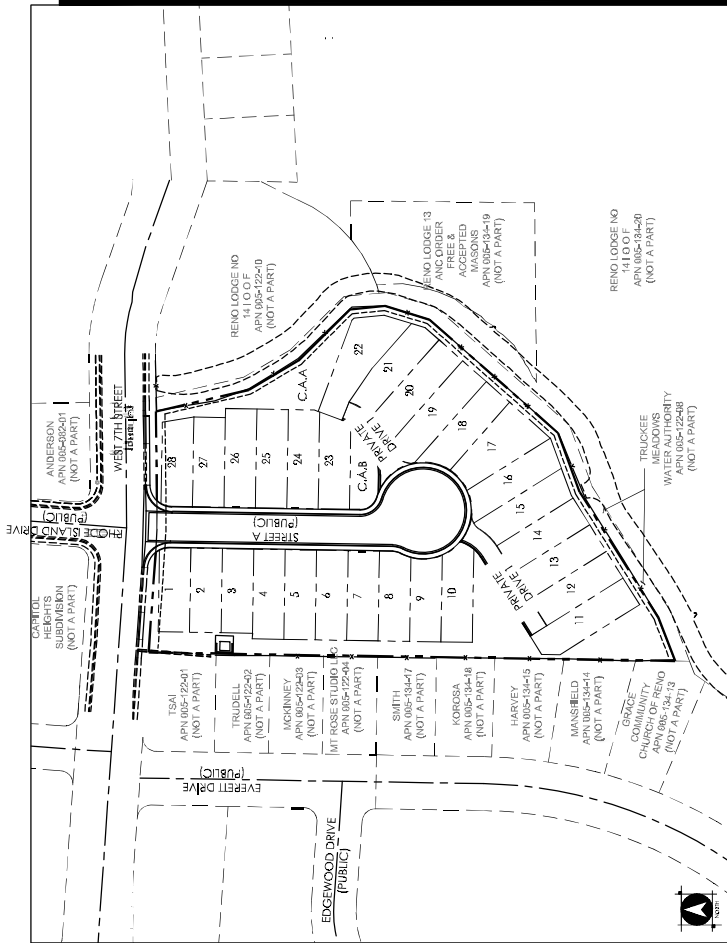
BASIS OF ELEVATION

U.S. GEOLOGICAL SURVEY (USGS) ONE METER 25'x48" IN REINO CARSON GEL 3817 U.S. GEOLOGICAL SURVEY.



PROJECT SITE

VICINITY MAP
NOT TO SCALE



SITE PLAN
NOT TO SCALE

ENGINEER'S STATEMENT:

I, ASHLEY VERLING, A PROFESSIONAL ENGINEER, HAVE PREPARED THIS PLAN AND SPECIFICATIONS FOR THE TENTATIVE MAP SUBMITTED AND WAS COMPLETED ON THE 22nd DAY OF JULY, 2024.

ASHLEY VERLING
P.E. No. 102779

SHEET INDEX

SHEET NO.	DWG. ID	DRAWING DESCRIPTION
1	T-1	TITLE SHEET
2	DM-1	DEMATION PLAN
3	UL-1	UTILITY PLAN
4	UL-1	UTILITY PLAN
5	G-1	PRELIMINARY GRADING PLAN
6	G-1	PRELIMINARY GRADING PLAN
7	CS-1	PRELIMINARY CROSS SECTIONS
8	CS-1	PRELIMINARY CROSS SECTIONS
9	LI	PRELIMINARY LANDSCAPE PLAN

2400 W. 7TH STREET
TITLE SHEET



WOOD RODGERS
ENGINEERING, INC.
BUILDING RELATIONSHIPS UNDERSTOOD AT A TIME
881 Corporate Boulevard
Reno, NV 89505
Tel 775-858-4088
Fax 775-858-4086

4402.001

JULY, 2024

SHEET T-1 OF 7

RENO LODGE 13 ANC
ORDER FREE &
ACCEPTED MASONS
APN 005-134-19
(NOT A PART)

NO 14100F
APN 005-122-10
(NOT A PART)

ANDERSON
APN 005-082-01
(NOT A PART)

- TRUCKEE MEADOWS
WATER AUTHORITY
APN 005-122-08
(NOT A PART)

CAPITOL
HEIGHTS
SUBDIVISION
(NOT A PART)

GRACE
COMMUNITY
CHURCH OF RENO
APN 005-134-13
(NOT A PART)

GRACE
COMMUNITY
CHURCH OF REN
APN 005-134-13
(NOT A PART)

APN 005-134-15
(NOT A PART)

APN: 005-134-18
(NOT A PART)

APN 005-134-17
(NOT A PART)

STUDIO LLC
APN 005-122-04
(NOT A PART)

MCKINNEY
APN 005-122-03
(NOT A BART)

TRUDELL
APN 005-122-02

TSAI
APN 005-122-01

A diagram showing a trapezoid with dashed lines extending from the top base to the bottom base. The left extension is labeled 4734 and the right extension is labeled 4736.

AF (N)

ASHLEY N. VERLING

JULY, 2024

SHEET DM-1 OF 7

KEY NOTES:

1	UNLOADING OF ACQUIRING	TO BE REMOVED
2	4" (102 mm) OF 2" (51 mm) C.C. CONCRETE	TO BE REMOVED
3	1" (25 mm) OF 2" (51 mm) C.C. CONCRETE	TO BE REMOVED
4	1" (25 mm) OF POLISHED CURB AND GUTTER	TO BE REMOVED
5	4" (102 mm) OF 2" (51 mm) C.C. LANDSCAPE WALL	TO BE REMOVED
6	1" (25 mm) OF 2" (51 mm) C.C. LANDSCAPE WALL	TO BE REMOVED
7	EXISTING GAS METER	TO BE REMOVED
8	EXISTING GAS METER	TO BE REMOVED
9	EXISTING GAS METER	TO BE REMOVED
10	EXISTING GAS METER	TO BE REMOVED
11	EXISTING GAS METER AND POWER	TO BE REMOVED
12	EXISTING GAS METER AND POWER	TO BE REMOVED
13	EXISTING GAS METER AND POWER	TO BE REMOVED
14	EXISTING GAS METER AND POWER	TO BE REMOVED
15	EXISTING GAS METER AND POWER	TO BE REMOVED
16	EXISTING GAS METER AND POWER	TO BE REMOVED

LEGEND:

A.C. PAVING
TO BE DEMOLISHED

P.C.C. CONCRETE
TO BE DEMOLISHED

EXISTING BUILDING
TO BE DEMOLISHED
(29.066 S.F.)

120

2400 W. 7TH STREET

TENTATIVE MAP

PRELIMINARY UTILITY PLAN

RENO LODGE 13 ANC
ORDER FREE &
ACCEPTED MASONS
APN 005-134-19
(NOT A PART)

RENO LODGE
NO 14 I O F
APN 005-122-10
(NOT A PART)

RENO LODGE
NO 14 I O F
APN 005-134-20
(NOT A PART)

ANDERSON
APN 005-082-01
(NOT A PART)

TRUCKEE MEADOWS
WATER AUTHORITY
APN 005-122-08
(NOT A PART)

CAPITOL
HEIGHTS
SUBDIVISION
(NOT A PART)

TSAL
APN 005-122-01
(NOT A PART)

TRUDELL
APN 005-122-02
(NOT A PART)

MCKINNEY
APN 005-122-03
(NOT A PART)

MT ROSE
STUDIO LLC
APN 005-122-04
(NOT A PART)

SMITH
APN 005-134-17
(NOT A PART)

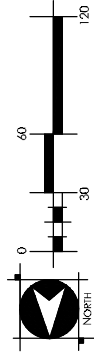
KOROSA
APN 005-134-18
(NOT A PART)

HARVEY
APN 005-134-15
(NOT A PART)

MANSFIELD
APN 005-134-14
(NOT A PART)

GRACE
COMMUNITY
CHURCH OF RENO
APN 005-134-13
(NOT A PART)

NOTE:
ALL STREETS ARE PUBLIC UNLESS OTHERWISE NOTED.
ALL STORM DRAINAGE IS PUBLIC (I/O.A.).
ALL WATER IS PUBLIC (I/O.A.).



2400 W. 7TH STREET
PRELIMINARY UTILITY PLAN

WOOD-RODGERS
BUILDING RELATIONSHIPS UNDERSTOOD AT A TIME
381 Corporate Boulevard Reno, NV 89502
Tel 775.858.4066 Fax 775.858.4066
JULY, 2024
4402.001
SHEET U-1 OF 7



NOTE: ADD 4700' TO ALL ELEVATIONS

RENO LODGE
NO 14100F
APN 005-134-20
(NOT A PART)

RENO LODGE
NO 14100F
APN 005-122-10
(NOT A PART)

ANDERSON
APN 005-082-01
(NOT A PART)

TRUCKEE MEADOWS
WATER AUTHORITY
APN 005-122-08
(NOT A PART)

CAPITOL
HEIGHTS
SUBDIVISION
(NOT A PART

2400 W. 7TH STREET
PRELIMINARY GRADING PLAN



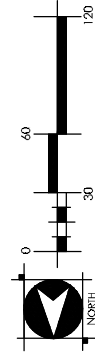
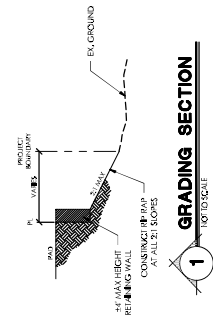
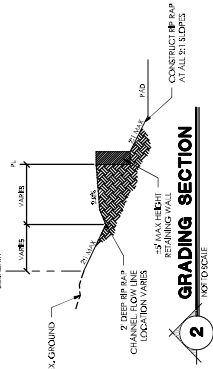
WOOD RODGERS
BUILDING RELATIONSHIPS OVERGROWN AT A TIME

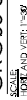
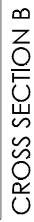
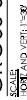
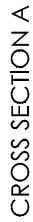
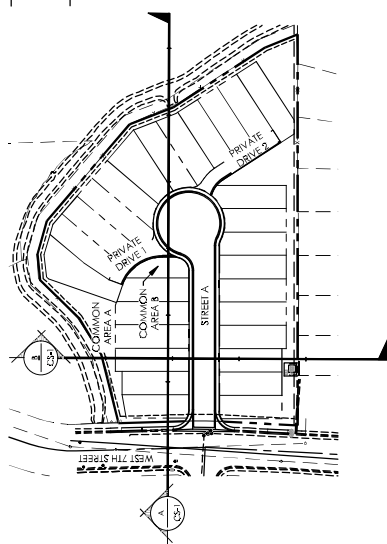
1361 Corporate Boulevard
Reno, NV 89502

Tel 775.828.4068
Fax 775.828.4066

4402001 JULY 2024

SHEET G-1 OF 7





WOOD ROGERS
BUILDING RELATIONSHIPS ONE-TO-ONE AT A TIME
1361 Corporate Boulevard Tel 775.828.4068
Reno, NV 89502 Fax 775.828.4066

4402.001 JULY, 2024

SHEET CS-1 OF 7

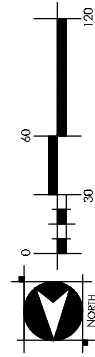


Exhibit C - Site Photos



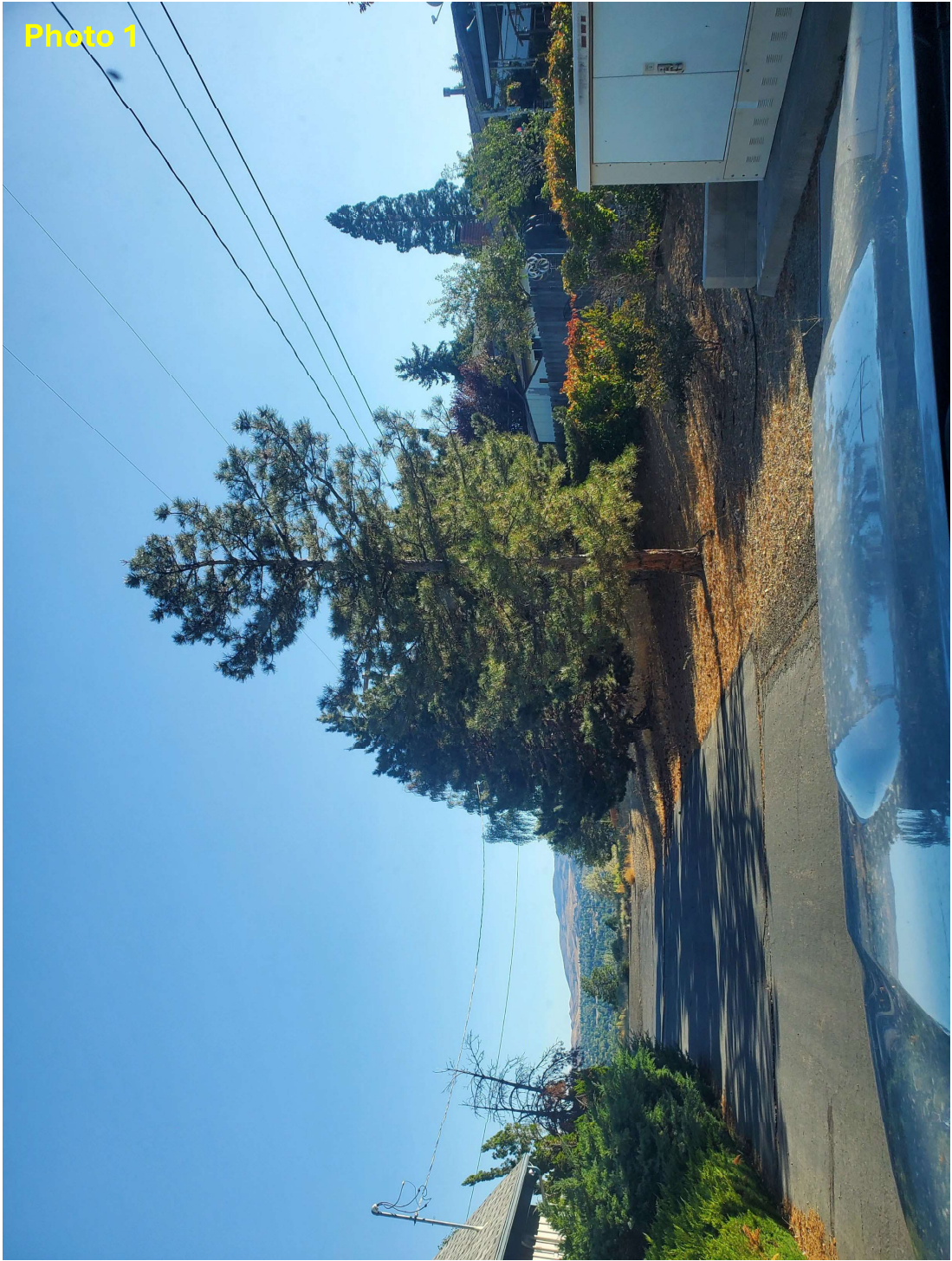


Photo 1



Photo 3

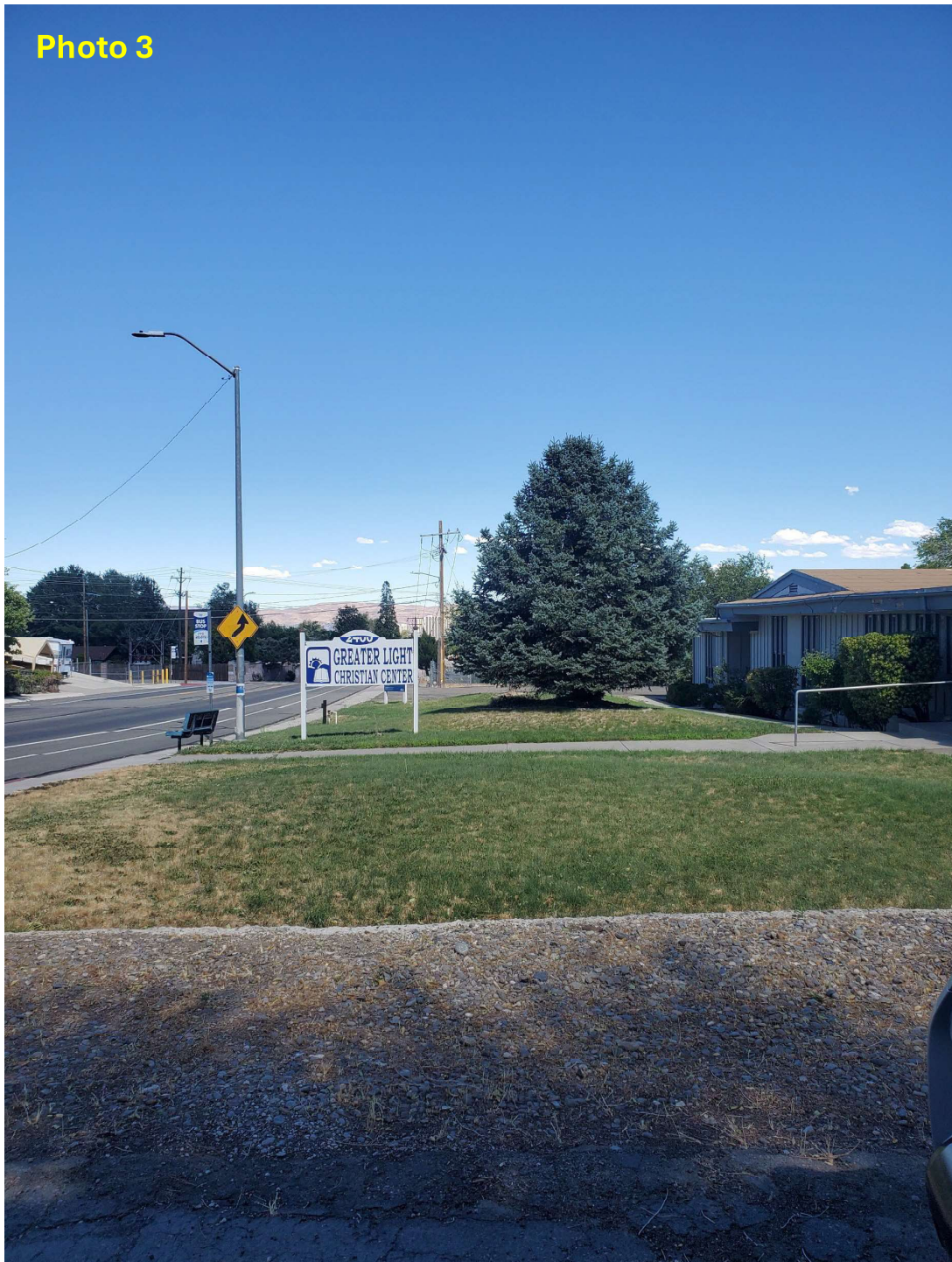




Photo 4



Photo 5

September 26, 2024

Leah Piccotti
City of Reno
1 East 1st Street
Reno, Nevada 89501

Re: **2400 W 7th Street Tentative Map (LDC25-00003)**
Justification on Meeting the Definition of Cluster Development

Leah,

This letter is to address the definition of a cluster development according to the City of Reno Municipal Code RMC 18.04.903(a) which states:

Cluster Development is encouraged to support the protection of sensitive natural resources, viewsheds, or other unique site features; promote fire safety within the wildland interface; provide opportunities for shared common open space; protect documented wildlife corridors;

Response:

The project is designed address the above definition by providing better viewshed protection than non-clustered development through a combination of increased setbacks/buffering and elevation change. Further, the project design proposes improvements to help meet unique site features to address off site drainage constraints. These improvements are provided in the proposed shared common open space which will also include amenities to help make the common open space more usable for the residents.

Viewsheds:

To better support the protection of the viewsheds for the adjacent neighbors to west, the project has been designed to allow the proposed lots to be set back further than code requirements, while still meeting the minimum setbacks of the underlying zoning. A common area buffer ranging from sixteen foot (16 ft) to twenty-two foot (22 ft) is provided between lots 1-10 and the adjacent neighbors. Furthermore, the proposed homes provide the minimum required setbacks, resulting in an overall increased separation of twenty-eight feet (28 ft) to forty-three feet (43 ft) from the adjacent properties to the west as demonstrated in Figure 1. When considering the viewshed, the buffer and the location of the proposed homes not only sets the houses farther away from the western property line, it also allows the homes to be lower in elevation than the existing homes, further helping to protect the existing viewsheds, as compared to



Figure 1

traditional lot development. Please note, if traditional, non-clustered lot standards were employed, proposed homes could be located as close as 20-feet to the western boundary of the project.

The grading is proposed to generally slope from west to east; therefore, the further east the proposed lots are located, the lower in elevation they can be set. As proposed, the finished grade of the houses are at an elevation that is between approximately nine feet (9 ft) to nineteen feet (19 ft) lower in elevation than the adjacent finished grades of homes along Everett Drive. This will allow the proposed two-story homes to sit lower than the elevation of the existing homes. The proposed homes are currently designed to be twenty-seven feet (27 ft) tall (less than the 35-foot maximum code height) as demonstrated in the cross sections in Figure 2. With the graded elevation difference, the perceived building height of the proposed 2-story homes will be that of a single-story structure. This combination of increased vertical and horizontal separation from the existing homes serves to better protect the viewshed than traditional lot development.

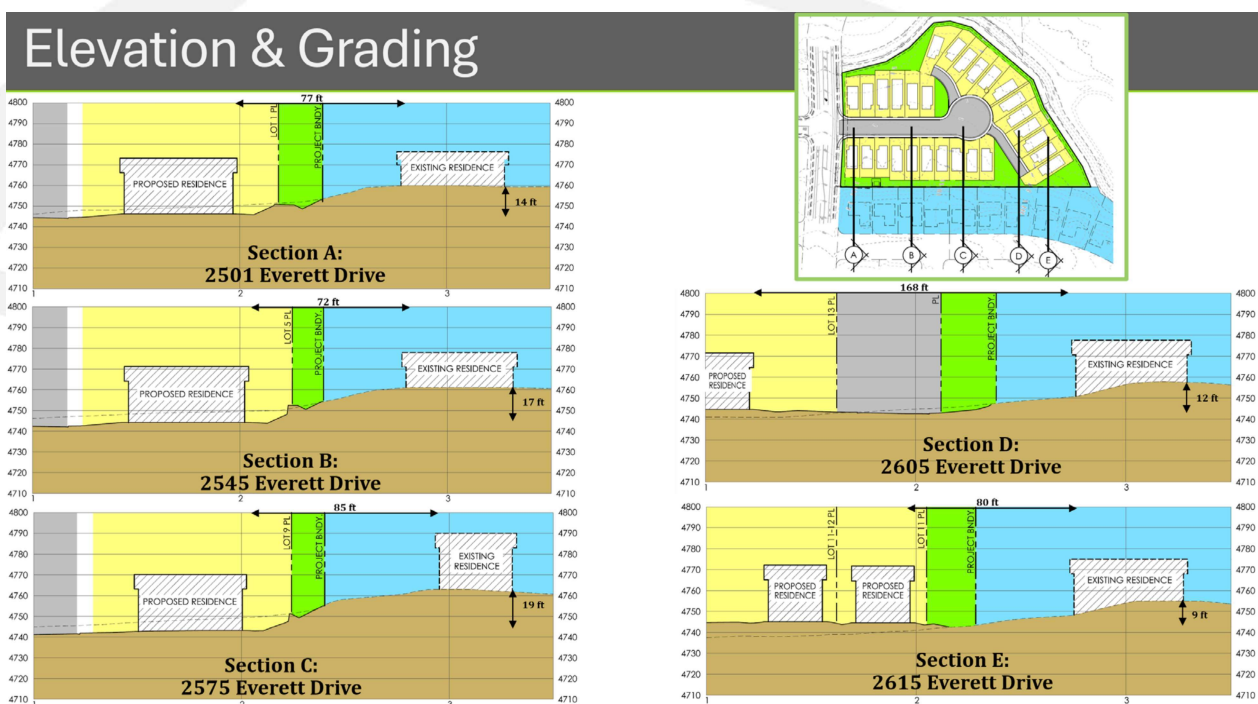


Figure 2

Unique Site Features:

The site is unique, as it must accommodate drainage coming from the developed lots to the west and draining onto the property. In addition, per the requirements of the Truckee Meadows Regional Drainage Manual (Section 305) the developed site is not permitted to continue to drain to the Highland Canal to the south and east as it historically does. Therefore, onsite improvements to accommodate for the onsite drainage must be provided to capture the onsite flows as well as any offsite flows from the adjacent properties to the west. These flows would traditionally be collected onsite and discharged to an adjacent storm drain facility in the public right-of-way. However, W. 7th Street does not have existing storm drain facilities adjacent to the project site which, in turn, requires retention ponds, in lieu of detention ponds to be provided on site. Therefore, the combination of the offsite flows from the adjacent property, the inability to continue to drain to the Highland Canal, and the lack of storm drain facilities in W. 7th Street is driving the need to utilize the common open space area to provide a drainage channel along the western boundary to capture the flows from the existing properties to the west and the utilization of an onsite

retention basin where the water from the surrounding properties and the water collected onsite can be allowed to percolate into the ground without discharging into the Highland Canal or W. 7th Street.

Typically, with infill development, the entire property can be used for development and does not need to accommodate for capturing flows from surrounding properties. This is typically seen at developments located on the boundaries of town in the outer and foothill neighborhoods where the land around the project is not developed and surrounding flows drain onto the property. However, this project must provide drainage facilities to capture these flows on an infill site. The proposed retention facilities accommodate for the unique site features while supporting protection of the viewshed but limits the opportunity for usable common areas.

Common Area Open Space:

As described above and throughout the materials submitted with this application the common area provided will allow for a landscaped buffer to provide a better transition between the existing lots to the west and the reduced lot size proposed in this request. This buffer will provide additional trees to enhance this transition and help to mitigate any potential impacts. The common area will also provide onsite retention ponds and drainage channels for capturing and conveying onsite, as well as offsite flows. To provide amenities that would allow the common areas to be more usable, two areas have been identified. The areas to the south of Lot 10 and to the west of Lot 11 will allow for usable common area amenities. The applicant has agreed to provide a gathering space and benches to create a usable amenity for the community to enjoy. An example of this is provided in Figure 3 below.

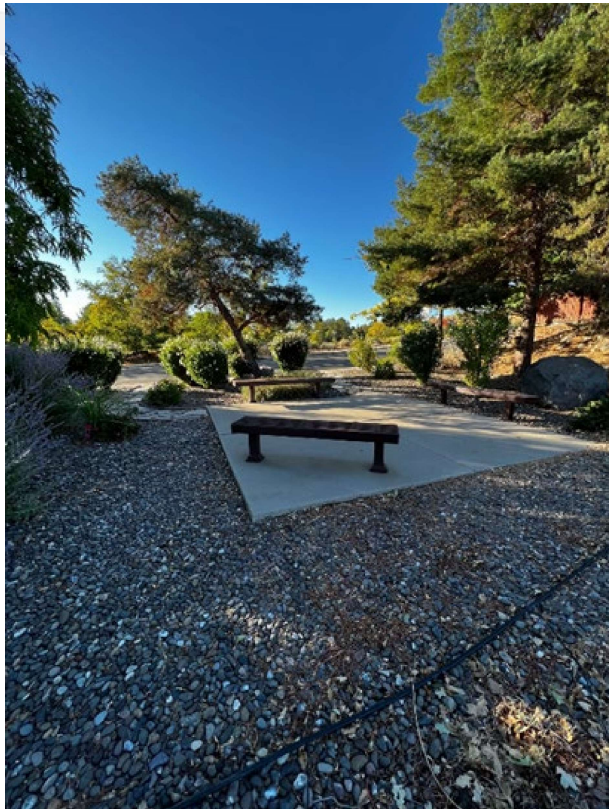


Figure 3

In addition to the amenities, the common area will be landscaped with trees and shrubs. It is intended that any viable evergreen onsite and particularly along the western boundary be protected. However, with the concerns listed in previous correspondence this may not be feasible and will be determined at

final map. New trees within the buffer and throughout the common area will help to make the common area a usable amenity the community can enjoy.

Should you have any questions or wish to discuss this further, please do not hesitate to contact me at ehasty@woodrogers.com or 823-9770.

Sincerely,

WOOD RODGERS, INC.



Eric Hasty, AICP



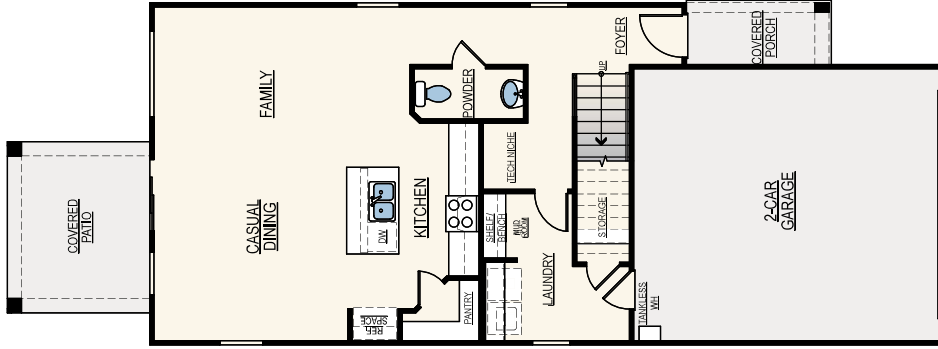
25' Series - American Samoa		
AH4R Asset Mgmt SF		1851
Plan Width		25'
Plan Depth w/o Patio		52'
Foundation Type		Slab/Crawl
Stories		2
Bedrooms		3
Baths		2.5
Garage Load		Front
Garage Type		Attached
# Garage Bays		2
AH4R Asset Mgmt Bsmt SF		N/A
Opt 3rd Car Garage		12'x20'
Opt Covered Patio/Lanai		12'x10'
JDE#		120
Legacy #		25-1857
Version		v1.0



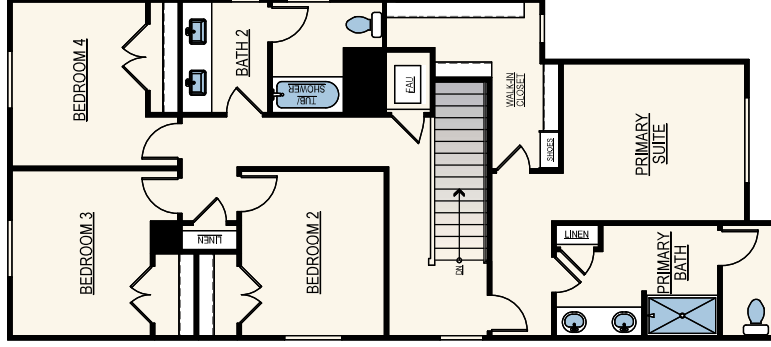
SECOND FLOOR PLAN
BASE ELEVATION SHOWN



FIRST FLOOR PLAN
BASE ELEVATION SHOWN



FIRST FLOOR



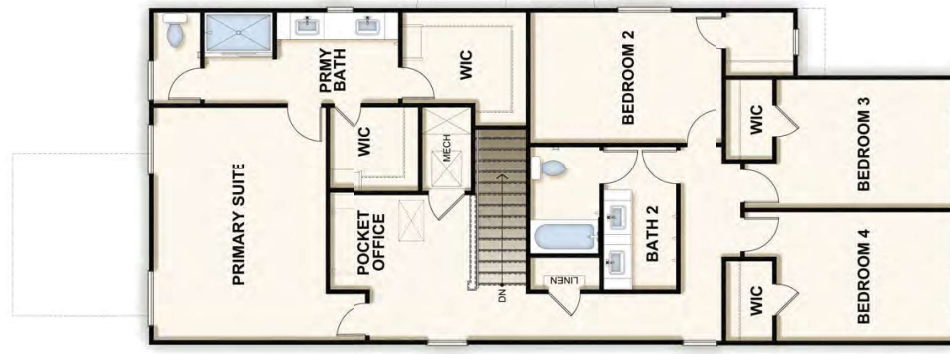
SECOND FLOOR

25' Series - Charleston Peak	
AH4R Asset Mgmt SF	1996
Plan Width	24'
Plan Depth w/o Patio	56'
Foundation Type	Slab/Crawl
Stories	2
Bedrooms	4
Baths	2.5
Garage Load	Front
Garage Type	Attached
# Garage Bays	2
AH4R Asset Mgmt Bsmt SF	N/A
Opt 3rd Car Garage	12'x20'
Opt Covered Patio/Lanai	12'x10'
JDE#	130
Legacy #	25-1986
Version	v1.0





FIRST FLOOR PLAN
BASE ELEVATION SHOWN



SECOND FLOOR PLAN
BASE ELEVATION SHOWN

25' Series -Black Mountain	
AH4R Asset Mgmt SF	2395
Plan Width	25'
Plan Depth w/o Patio	60'
Foundation Type	Slab/Crawl
Stories	2
Bedrooms	5
Baths	3
Garage Load	Front
Garage Type	Attached
# Garage Bays	2
AH4R Asset Mgmt Bsmt SF	N/A
Opt 2nd Car Garage	12'x20'
Opt Covered Patio/Lanai	12'x10'
JDE#	170
Legacy #	25-2400
Version	v1.0









A Tree on Your Property

It is the property owner's responsibility to keep trees clear of their electric service line. This is the line that runs from the pole to the house.

NV Energy will prune trees that grow too close to high voltage power lines. These power lines run from pole to pole. It is always a good idea to call NV Energy to inspect any trees that grow close to any power lines. The clearance space required increases as the voltage increases.

Trees don't need to be touching the power lines to require pruning. They will be pruned if they are growing into the clearance space. Achieving the required clearance may mean pruning on your property.

Why Not Just Shape Trees?

Shaping was once a popular practice with tree pruning, but it has now been learned that this is not horticulturally correct, as it can create a safety hazard to both your property and overhead lines. Shaped trees re-grow quickly with vertical suckers that sprout up from cut branch ends. The suckers are only weakly attached to the branches and are more likely to break off, even in only moderate winds, creating a greater problem of windborne debris. This can lead to power blackouts and the need for far more frequent tree pruning. Proper pruning may look severe at first but trees will re-grow at a normal rate and are trained away from power lines. Tree health is maintained and even enhanced by correct cutting at the branch collar.

You Can Help

Customers can help reduce costs and maintain a pleasant environment by ensuring trees of the right size are planted under and around power lines. Remember *"Right Tree, Right Place."*

Seek permanent solutions. Do not plant trees near electric lines; or plant only low-growing, compact varieties. The guidelines given here aim to reduce sprouting, reduce the working time in a tree for the tree worker, increase the time between pruning cycles, and help the trees planted near lines to be healthy, safe and as attractive as possible. It is better for the trees, lines and customers to remove fewer but larger branches at nodes. Nodes are where branches shed naturally. It will take time to make all the adjustments given here. But, if we all work together, the adjustments will come. And the trees, the tree workers, the electric power companies, and the electric power users will all benefit.



IMPORTANT NV ENERGY CONTACT INFORMATION

NV Energy Tree Pruning Request Line

Northern (775) 834-4444

Southern (702) 402-5555

nvenergy.com/tree

Automated Power Outage and Emergency Service

Northern (775) 834-4100

Southern (702) 227-2900

Customer Service

Northern (775) 834-4444

Southern (702) 402-5555

En Espanol

Northern (775) 834-4700

Southern (702) 402-5554



Why Does NV Energy Prune Trees?

CUSTOMER SAFETY



Exhibit F - NV Energy Tree Trimming Explained



rev 2017/05

Why Does NV Energy Prune Trees?

NV Energy's Tree Pruning Program is intended to help protect the public, protect property and maintain electric reliability. At NV Energy, trees are a cause of power outages. Short circuits from branches rubbing against power lines, branch breakage and trees blown over during storms are just a few of the hazardous situations that can occur.

Safety

Tree pruning is an essential part of maintaining a safe and reliable supply of electricity for your convenience. Attempting to prune or climb a tree near power lines can be dangerous. Each year, hundreds of people throughout the United States are injured or killed when they climb or prune trees. Concealed power lines can seriously harm children climbing in the branches. Power lines broken by falling branches are extremely dangerous and may cause fire, shock or electrocution. By pruning trees and brush we can reduce tree-related outages and eliminate safety hazards. Please remember that any trees interfering with utility power lines can lead to a dangerous situation.

Legislation

Because tree pruning is so important, legislation exists which requires both NV Energy and the property owner to ensure that a minimum clearance space between trees and power lines is maintained.

Why Does NV Energy Prune Trees on Private Property?

Sometimes trees will grow to reach NV Energy lines. Under these circumstances NV Energy will prune branches to prevent damage and danger to the power lines from swaying branches.

Who Does The Work?

NV Energy uses skilled and specially trained staff and selected contractors who have received training to meet the requirements given by OSHA and ANSI Z 133.1–2000.

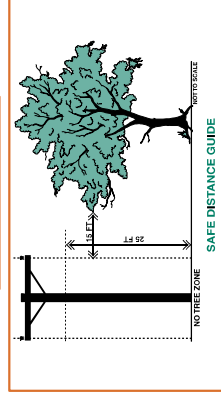
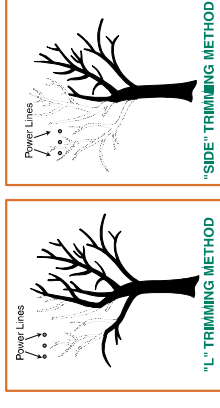
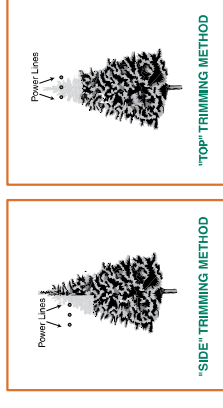
How Do We Prune Trees?

At NV Energy we follow the work practices recommended by *The International Society of Arboriculture; ANSI A300 2001 Tree Pruning*, and described by Dr. Alex L. Shigo in his book, *Pruning Trees Near Electric Utility Lines: A Field Pocket Guide For Qualified Line-Clearance Workers*.

These practices include:

1. Techniques consistent with the practices of natural, lateral and drop crotch pruning.
2. Cuts are made back to a main stem or branch of sufficient size. In no case shall deciduous tree limbs be stubbed off at the edge of the clearing limits.
3. Wherever possible, all pruning cuts shall be made to direct future growth and sprouting away from the conductors.
4. Conifers should be pruned in a manner that allows them to retain as much of their natural shape as possible.

TREE PRUNING EXPLAINED PRUNING METHODS



5. All dead branches overhanging primary conductors at any height shall be removed.
6. A minimum of cuts should be utilized to achieve required clearances.
7. Where practical, cuts should be primarily restricted to large diameter branches made well within the crown. Shaping through the use of many small diameter branches in the outer crown should be avoided.
8. Remove living branches by making cuts as close as possible to the branch collar. Remove dead branches by making cuts as close as possible to the living tissues that surround the dead branch at the base.
9. Precautions shall be taken to avoid stripping or tearing of bark when cutting large-diameter limbs.

Who Has Responsibility to Make Sure Trees are Clear from Power Lines?

Everyone. Although NV Energy owns, maintains, inspects, and prunes around power lines at regular intervals, it is impractical for NV Energy to address trees growing at different growth rates on a day-to-day basis throughout the valley and surrounding areas. NV Energy not only relies on its own employees, but also property owners, customers, and tree pruning service professionals to relate information about trees that are close to or even in contact with power lines. Since safety is NV Energy's first concern, there is no cost to prune or remove trees when they are growing close, could make contact or could fall into the power line and create a hazard.

Exhibit G - Front & Side Setbacks & Stepbacks

Chapter 18.04 Development Standards

Article 9 Site and Building Standards for Residential Districts

18.04.903 General Standards for Residential Districts

a. Front Setbacks

A 2:1 building height/setback ratio shall be maintained from all front property lines (applied to the portion of building fronting the property line, not the entire building)(see Figure 4-19, below).

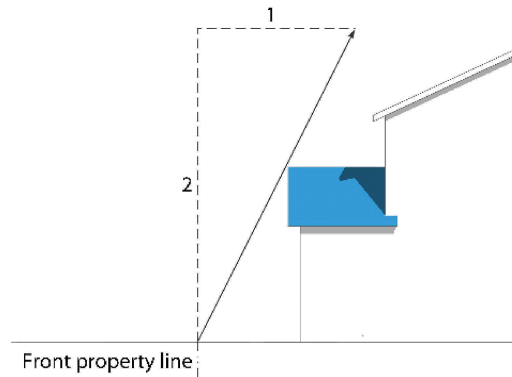


Figure 4-19: Front Setback

b. Side Setbacks

1. Any building abutting a property zoned LL (all districts), SF (all districts), MF-14, or MD-PD shall comply with the minimum side building setbacks of the abutting residential zoning district along the common property line (see Figure 4-20, below).
2. Any portion of any building that exceeds 24 feet in height shall maintain an

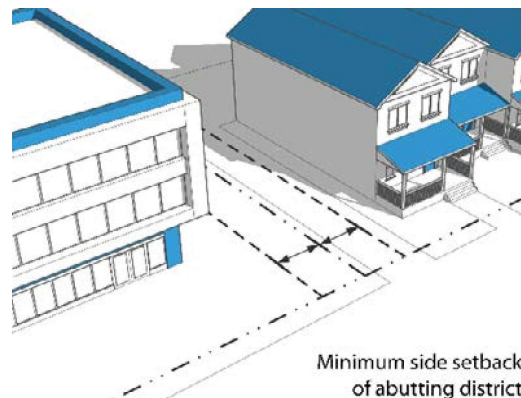


Figure 4-20: Side Setback

additional one-foot side-yard setback for each foot of height over 24 feet (see, Figure 4-21 below).

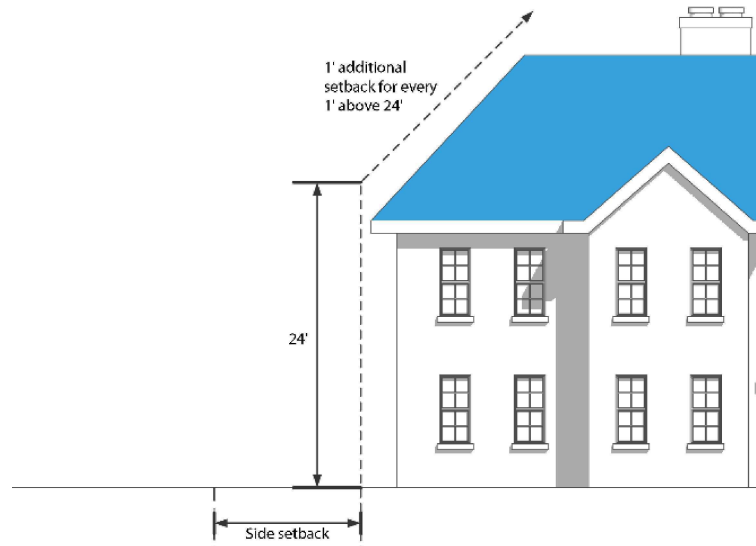


Figure 4-21: Side Yard Stepback

3. Within the rear setback and the larger of the front setback or 15 feet from any front property line, portions of any building that exceed 12 feet in height shall maintain an additional one-foot side yard for each foot of height over 12 feet (see Figure 4-22, below).



REGIONAL TRANSPORTATION COMMISSION

Metropolitan Planning • Public Transportation & Operations • Engineering & Construction

Metropolitan Planning Organization of Washoe County, Nevada

August 9, 2024

Leah Piccotti
Development Services
City of Reno
1 East First Street
Reno, NV 89501

RE: 2400 West 7th Street – Major Site Plan Review –LDC25-00003 – RTC Comment Letter

Dear Ms. Piccotti,

RTC appreciates the opportunity to comment on the proposed 2400 West 7th Street project located in Reno. RTC is committed to working with City staff, developers, and other stakeholders across Washoe County on transit-supportive developments that grow ridership, reduce driving, and promote walkable neighborhoods.

The purpose of this letter is to make comments ensuring that the Project is in compliance with approved RTC plans, programs, and initiatives, and to provide recommendations based on the project's proximity to any RTC existing or upcoming roadway improvements and/or transit services.

2050 Regional Transportation Plan (RTP)

This project impacts a regional road, West 7th Street, as identified as an arterial with a moderate access control as identified in Appendix D – Access Management in the [2050 RTP](#).

The Policy level of service (LOS) standard for West 7th Street is LOS D.

Active Transportation & Transit Orientation

RTC Bus Line 4 stops on West 7th Street, adjacent to the proposed project. RTC Public Transit Operations recommends that the bus stop be moved temporarily during construction, and that once completed, the current stop location is maintained and improved to comply with ADA standards. Please see the document attached for reference to best practices. Please contact Sai Sun at 775-335-1904 or ssun@rtcwashoe.com with any questions.

RTC supports the goals and principles outlined in the Reno Master Plan, which emphasize mixed-use, transit-oriented development and community revitalization projects that encourage walking, bicycling, and easy access to transit. In order to enhance walkability, bikeability, and transit use, the City should consider requiring installation of wide sidewalks, pedestrian lighting, ADA-compliant curb ramps, and easily accessible bike racks as a condition of project approval.

Additionally, RTC encourages the incorporation of pedestrian-oriented building design strategies such as placing of building facades along the sidewalk, locating surface-level parking behind the building and away from walkways, and strategically including entrances and windows facing the street for convenient pedestrian access.

RTC looks forward to reviewing any further documents related to this project. If you have any questions regarding this response, please contact Marquis Williams by phone at 775-332-0174, by email at MWilliams@rtcwashoe.com, or by mail at the following address:

RTC Development Review
1105 Terminal Way, Suite 211
Reno, NV 89502

Sincerely,

A handwritten signature in black ink, appearing to read 'Marquis Williams', written in a cursive style.

Marquis Williams
Senior Technical Planner

Attachment:

- RTC Bus Pad.pdf

Figure F. New Small Pad - 15'x3' pad

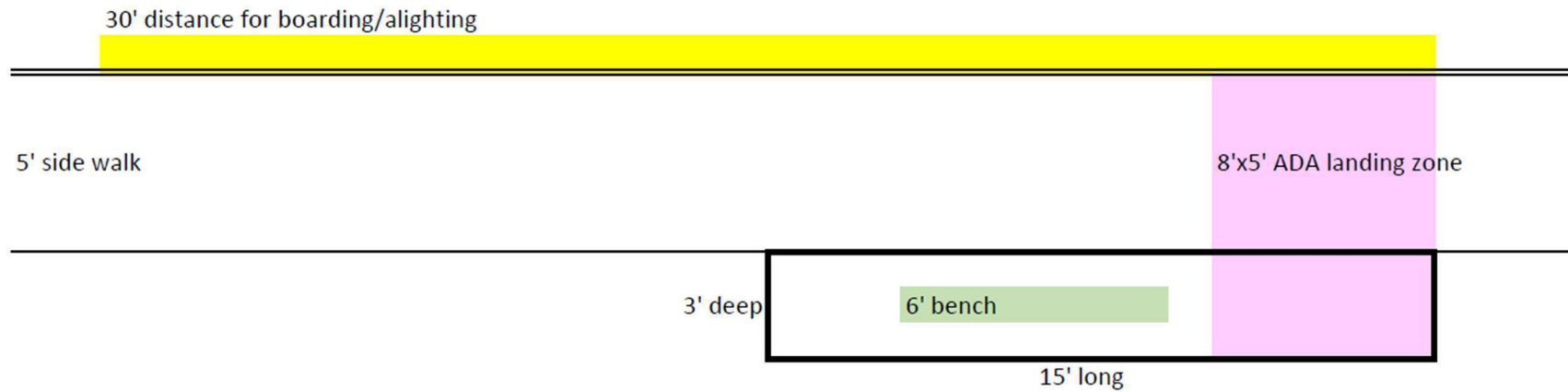
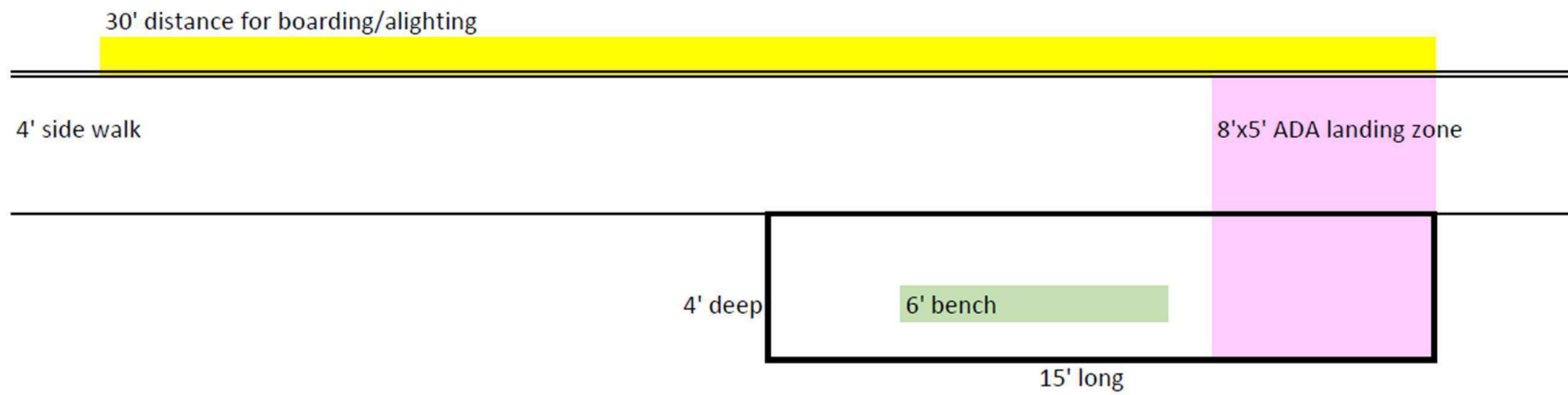


Figure G. New Small Pad - 15'x4' pad





Development Review Public Comment

The public comment form has a new entry from the public.

Case Number

LDC25-00003

Position

In Opposition

Comments

I would like to see plat map of how 28 units in the subdivision will fit. Dividing the land by 8 units/acre is less than 5500 SQFT. This doesn't take into consideration roads and other offsets. There also does not appear to be a secondary emergency access from this area. It is blocked on two sides by the ditch and a third by a subdivision that has been there since the late 60s. I understand Reno needs housing, but as listed, this doesn't make sense.

Email Address

greyjess1@yahoo.com

Name of Commentor

Jessica Goza-Tyner

Phone Number

775-671-1602

Submitted: 7/30/2024 8:21:13 PM

These comments were submitted on behalf of: family, Genn and Judy Goza (self if blank)

Development Review Public Comment

The public comment form has a new entry from the public.

Case Number

LDC25-00003

Position

In Opposition

Comments

I DO NOT SUPPORT A DEVELOPMENT OF THIS SIZE. THE SURROUNDING NEIGHBORHOOD LOT SIZES ARE ABOUT 7,000 S.F. PLUS. THE PROPOSED DEVELOPMENT LOTS HOVER AROUND 3,500 S.F. 28 ADDITIONAL HOUSEHOLDS IN A TIGHT CORRIDOR ON AN ALREADY BUSY 7TH STREET IS NOT WISE, I WOULD BE IN FAVOR OF LOTS OF EQUAL SIZE TO THE SURROUNDING ALREADY ESTABLISHED LOTS AND SOMEWHERE AROUND 14 HOUSEHOLDS. A DEVELOPMENT OF THIS SIZE SIGNALS THAT THE DEVELOPER DOESN'T CARE ABOUT THIS NEIGHBORHOOD, AS NO ATTEMPT TO BLEND INTO THE SURROUNDING ELEMENTS HAS BEEN MADE, I AM NEVER OPPOSED TO THOUGHTFUL DEVELOPMENT. HOWEVER, THIS IS NOT THOUGHTFUL; SMELLS OF GREED AND HOME QUALITY WILL BE THAT OF MASS-PRODUCED TRACT HOMES. CUTTING AS MANY CORNERS AS POSSIBLE FOR THE SAKE OF MORE MONEY, NOT A FAN.

Email Address

frankpulido@gmail.com

Name of Commentor

FRANK PULIDO

Phone Number

7752195916

Submitted: 8/9/2024 7:39:16 AM

These comments were submitted on behalf of: (self if blank)

Mrs. Piccotti,

I would like to thank you in advance for your time and consideration. As I am aware of personal biases I carry – I'll begin by addressing those before I move on to public and safety concerns.

When 2605 Everett came up for sale, I was captivated by the view and called my husband immediately. It was clear from the photos what rough shape the home was in, but the view had our hearts instantly. We did our due diligence asking the neighbors and the Church about the view, if there were any plans to build to which were met with resounding "no's". We were told about how at one point the church had tried to expand and that the city of Reno shut it down because you could not safely get a firetruck inside and traffic concerns. All of which stand in my opinion, potentially even more now that the area has grown. So, we moved forward and overcame many obstacles to be here, including the fact that the house was in such poor condition from squatters living here that we could not secure a loan and had to borrow privately to complete closing.

We are a middle-class family who did not have the budget to buy the home and pay for it to be remodeled. We did however have a vision and a great mind for DIY. We have lovingly spent the last 7 years of our lives turning this house into a home. I've attached photos for you to see the "before" and all we have done. We have truly built this house, the deck, the yard – everything around enjoying, looking at, and loving our beautiful city we call home. All the neighbors have as well.

Cluster developments are meant to not inhibit home value as far as my understanding, and each of our homes will take a huge equity hit if you factor in that our neighbors spent an additional \$30,000 view premium just two short years ago.

That being said, I do understand business and that I do not own the property behind our house. Even still I always dreamed that one day the Church could expand and do a garden, or a school, or even that one day a park there for our kids to play. As we previously lived in Sparks before moving to Reno, we were stunned at the lack of parks for neighborhood kids to play in. The nearby schools have fences and restricted access outside of school hours.

In addition, I have grave concerns not as a homeowner, but as a citizen. Before the purchase of 2605 Everett Dr. we lived on 3390W 7th street. It mirrors a similar bend in the road as the curve into the church parking lot, and only after moving in did we hear from the neighbors how dangerous the area was. Two houses down our neighbor had someone die in their front yard and could hardly remember how many times they were hit. We renovated that house as well and sold as soon as renovations were complete. The new owners house

has been hit at least four times. West 7th, we have been told, cannot add more roundabouts, stop signals etc. because it is a thoroughfare for emergency crew. I can say with resounding certainty that adding a minimum of 56 more cars (if you assume the average home owns 2 cars) turning around that blind corner will cause several instances.

Parking is another issue we have. In the plans its listed that street parking is available. This is neither true nor safe. No one parks cars on 7th street which will leave extra parking for guests up to neighborhoods, such as Everett.

Below I've attached a few images of the home in its condition at purchase, and now.

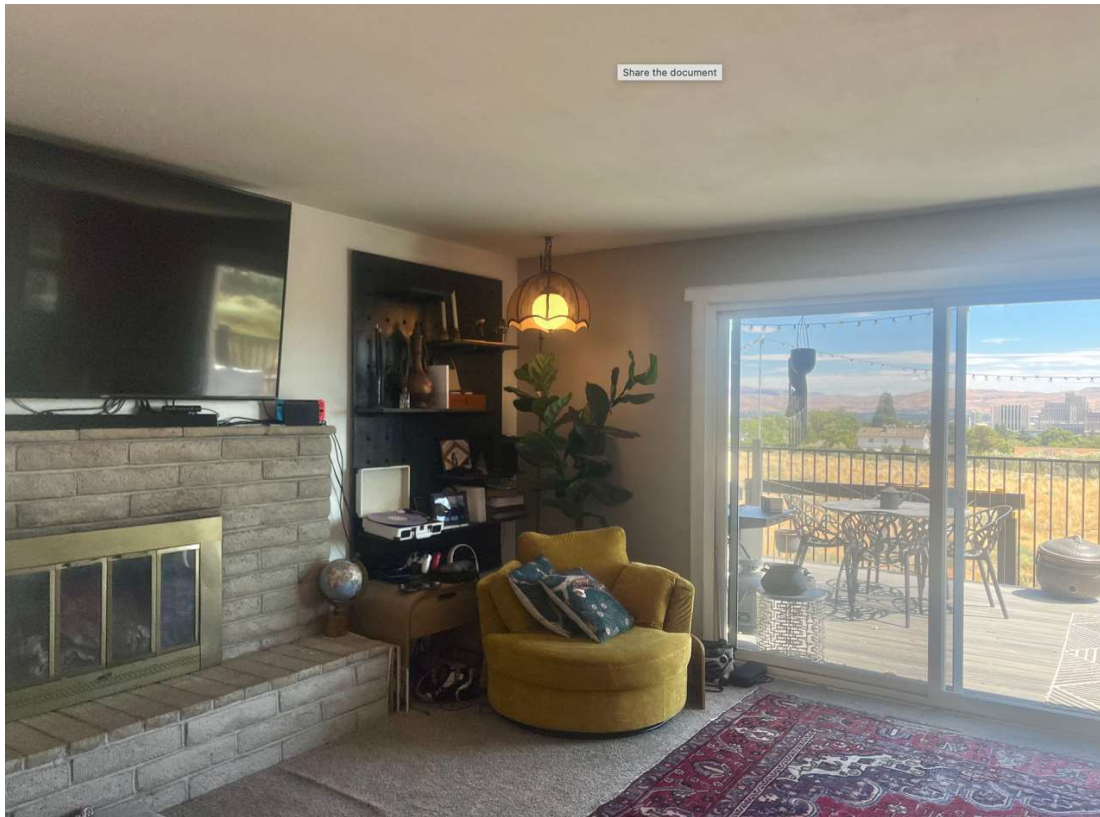
Thank you again for your time and consideration of the community. It means a lot to us.

Sincerely,

Ashleigh Harvey

775-225-4848







Leah Piccotti

From: Glenn Goza <reno-boy@hotmail.com>
Sent: Wednesday, August 14, 2024 10:21 AM
To: Leah Piccotti
Subject: Case Number LDC25-00003 comments

Leah – I was unable to attend last night's Neighborhood Advisory Meeting reviewing Case Number LDC25-00003, but I would appreciate if my concerns could be passed along to the members. There are only a total of two bus shelters and five bus benches between N. McCarran Blvd and Stoker Ave. One of these benches is positioned on Seventh Street in front of the church across the intersection of Rhode Island Drive and Seventh Street. If this project proceeds, I would ask that the bench be replaced as close as possible to its current location. In all fairness, it should also be placed nearer to any new construction rather than moving it to the property just west of the church. Also, TMWA does between four and five cleanings of the Highlands ditch during the year. The ditch on that side is not wide enough for a truck to use. For as long as I can remember, over 50 years, they have always parked for the several days involved each cleaning on a portion of the church's parking lot, done their work and removed the waste to the parking lot and then on to the truck(s) and/or trailer. The curb east of the church's parking lot entrance is red all the way past the cemetery's gate. This is due to it being a blind downhill curve. Where and how will they park? Will the light post on Seventh Street in front of the church be replaced in a similar spot?

Glenn Goza 840 Rhode Island Drive, Reno, NV 89503 775 747-2739

Leah Piccotti

From: Barbara Korosa <1sierralady@att.net>
Sent: Thursday, September 5, 2024 6:48 PM
To: Leah Piccotti
Subject: My presentation speech at CAB meeting

Leah,

I want to share with you the statement I prepared and was partially able to present at the most recent CAB meeting with the development as an agenda item. Due to the 3 minute presentation limitation, I was not able to present my entire statement. The below has more depth in facts than my previous email I sent to you and I request it is included in the material for the development.

Thank you.

Barb Korosa

I am here today to address the agenda item of the development of 2400 west 7th Street.

I would like to advise I am a resident of Reno for over 44 years. My previous residence I lived in for 38 years. I could go into more detail of my prior residence, its zoning, how the City zoned it and its uniqueness as it contained acreage, you can contact me personally for information.

I have been aware that the property in question has been for sale when the sign was placed on the property and of its current zoning. I have hoped another church would purchase the property. An interested private school was in negotiations as well, but were unable to come to an agreement. I would strongly support this type of use for the property. This area was developed in the 1970's. I have come to know many of the original property owners still live in their homes and are aging in place which is a significant fact to the area they want to live in. That fact strongly speaks to the neighborhood. There is also a transitioning of new, younger families purchasing and making these homes their desired area to live.

I understand the City, while working within current known zoning, tries to best fit the appropriate classification of an unusual sized property within the parameters of current zoning available for every parcel in Reno. When an unusual size of a parcel exists, size of the parcel determines actual zoning designation. I know of parcels that have existed when acreage was involved, and zoning, I personally believe, just doesn't fit, but whatever the practice is used by the city, it continues. In the example of the proposed plans, the designation of 28 units is trying to work within these parameters. The developer is citing a "cluster development" but by municipal code exceeds these standards by 42%. This fact alone negates the development proposal. When the Planning Commission reviews the proposal, it needs to be pointed out this glaring inaccuracy. If compliance according to Municipal Code prevails for this property as a "cluster development," the number of units would be significantly reduced.

Another extremely important concern is traffic. Approximately 1995, Montessori school wanted to partner with the church for a school on the property. 7th Street has been a main artery of this Northwest neighborhood from the inception of when this area was being developed in the early 1970's. In 1995 the number of people living in Reno was 156,000 - today according to the most recent census, and that number is higher but for purposes of stating population, is 274,915. The 1990's, growth was a huge factor to this valley. As part of the Montessori school request, a most needed Traffic Study was performed. It was determined that a safety risk for this development was deemed to exist and because of the safety factor of traffic then on 7th Street, the development was denied. I am repeating this fact again, the development was denied. 7th street is not able to change in any manner it's design and presents a huge concern for today's access and vehicles that travel on it. Just this morning, at 8 a.m., I was on 7th street and the number of vehicles on it was shocking. I do not normally drive during rush hour, but I noticed the traffic specifically this morning. In addition to people driving to work, I am sure these vehicles were parents taking their children to the schools also located off 7th Street. I am strongly emphasizing and pointing out, if this safety risk existed in 1995 with that population at that time, the current population is even more of concern. There is a huge difference of usage by a school versus, meaning access is much less, and the proposed housing with all likelihood of 2 vehicles per unit and 24 hour 365 day usage, safety becomes a major point of concern. Minimally a traffic study, independently done by a third party not associated with the developer, is to be required. 7th street has an elevation and curvature to the street just east of the property causing a risk to anyone driving on the road, and I drive on 7th street and is a personal safety concern to me. A neighbor pointed out to me when I first moved to this home, because 7th Street has a very unique elevation change, as well as a significant curvature of the road in the area I cited, any icy road conditions, 7th street is not safe to reasonably drive on. Granted icy road conditions are during cold months of the year, but still is a condition I consider as to the safety of this area. The facts stated, especially traffic and safety, cannot be ignored and needs to be addressed with even more importance.

Leah Piccotti

From: Barbara Korosa <1sierralady@att.net>
Sent: Monday, August 12, 2024 2:50 PM
To: Leah Piccotti
Subject: Proposed 7th Street project

Miss Piccotti,

This email is being provided concerning the 2400 West 7th Street proposed development.

I have lived in Reno for more than 44 years, 38 years of which I lived in one home that was an original ranch (along the Orr Ditch) with a “chicken barn” on the property. I have personally seen how this valley has grown and changed - some changes ok and quite a lot that I believe has hurt Reno.

My current home was a significant move for me personally and physically. I am a widowed, retired, single woman. I wanted a good sound home, which I believe this area was and homes were built with good construction values. Part of the consideration to purchase this home is my personal view of the valley. This view cannot be duplicated and was part of my decision to purchase this home. Yes, I have invested considerably for this to be a better home than when it was purchased, but views are something once you take away, they are gone forever and I believe, unfortunately, that has happened all too often.

There are several concerns in the proposed development in which Municipal Codes are not being followed and the Planning Commission should recognize and address before further work/review is done. Of particular concern is stating “Cluster” plan. It is clearly being violated by the developer.

A significant concern is traffic. I would like to point out, approximately 1995-96 a proposal for a Montessori school was to be built on the property - sharing with the church. A traffic study was done and this proposal was denied based on traffic safety concerns of a blind area on 7th Street to the development. Traffic in the 1990’s and now has significantly increased and at present, this development reflects the same concern. If it was denied previously, that still has the same, or more, validity.

Infrastructure is a critical component when density of an area is changed. A Traffic Study needs to be required and by a third party that has no association to the developer.

Thank you for the opportunity to present important information.

Sincerely,
Barbara Korosa

Leah Piccotti

From: ronnipitzer@sbcglobal.net
Sent: Friday, August 30, 2024 4:21 PM
To: Leah Piccotti
Cc: ahill@washoecounty.gov
Subject: Case Number LDC25-00003

Good Afternoon,

I live at 2545 Everett Drive and have several issues with this subdivision.

- 2 Story Houses with 5ft spacing between each building will take away my view of the city and valley and reduce the value of my house.
- Regardless of what the developers say. I'm sure the residents will have roommates who will be parking on side streets in the neighborhood.
- 7th Street traffic is already heavy and the entrance/exit to the development is an especially bad spot.
- I do believe the schools in the area are overcrowded now.

Is there still going to be a public hearing on 9/18/24 regarding above?

Hopefully, the planning commission has determined that the developers' criteria for determining that the site qualifies as a cluster development is invalid and the meeting has been canceled.

Thank you for your attention to this matter.

Veronica McKinney

2545 Everett Drive

Reno, NV 89503

(775)224-6719

Good afternoon Mrs. Piccotti,

We just spoke on the phone, I am writing to express some of the reasons we don't think this project is the best option for our neighborhood as well as does not align with the Master plan for the city, nor does it meet Reno Municipal code.

First, the dangerous traffic conditions on seventh street have been an issue for literally decades. Prior to moving to our current address my wife and I lived a half mile up the road on 7th street, we had cars totaled in front of our home due to drunk driving. I've seen a RTC bus go through a neighbor's fence just past Heights, and a little over 10 years ago a drunk driver went through a fence on Heights and 7th and the passenger lost their life by being impaled by a fence post. Multiple people further up 7th have added large boulders to keep themselves safe. Unfortunately, the impaired drivers are not limited to further up seventh, approximately one year ago a drunk driver went up over the grass and plowed through the GLCC Church's sign. If the homes proposed were there as now planned, that car would be in someone's living or bedroom. They are set only feet from that dangerous road. The curve and elevation change on 7th street makes it difficult to safely enter and exit from the proposed entrance to the development.

Second, what is this development adding to the community? Where will these kids play? There are zero parks within walking distance of our homes already and all the school playgrounds now have locked fences so kids cannot play their either. The development mentions common areas and designates them in certain drawings as grass and trees however in their 160 pg plan the same common areas are called retention ponds. Is this where the kids will play?

Third, RMC 18.04.903 defines Cluster Developments. After reading the definition this proposed development literally meets none of the criteria as far as I see. It simply seems that they are trying get around changing zoning by calling it a cluster development. There are no sensitive resources and there is no transition into unincorporated county or public lands. Even if it is a cluster development, number one the density increase can be no more than 15%, it is currently proposed to increase by 42%. Number two, there is the issue of parcel size matching, it's currently at 2 to 1 and the RMC states they need to match neighboring parcel sizes. There is the exception to utilize a buffer zone, however, even if this is implemented the buffer zone must be 30 feet if fully landscaped. Currently, their plan calls for mostly 17 feet on the western side with the exception of, behind my house, where for some reason it dips to only 5 feet. This is clearly not code. Additionally, there is the yard matching requirement where the RMC states rear yards must be the same width of the existing development. The submitted tentative site plan by Wood Rogers says throughout the plan that their new site plan "is compatible with surrounding developments". It clearly is not compatible and will not enhance home values in the area. If we lived in Sommer Set this would never be considered.

Lastly, the Reno Master Plan calls for more parks and community resources. The current church GLCC, is a pillar of the African American Community in Reno. Pastor Taylor has worked in Reno City Government and has been on different governing boards, and is a valuable member of our

community. After speaking with him, I don't know that he has an idea of where his church will meet if this development goes through. I do know that he has plans for that building and the property. What if instead of throwing up a densely constructed set of homes the city of Reno partnered with GLCC and built some open space behind the church? Perhaps a community garden, playground and picnic area to preserve the open space that has amazing city views. That type of development would be in line with what the Reno City Master Plan details.

In summary, I do not believe this development takes into consideration the overall good of the surrounding neighborhood but instead adds to the burden of the already limited resources in the area. Grace Warner Elementary is title one school where my youngest son attends. My daughter started her first day of her freshman year today at McQueen, it was built for 1200 students but is currently around 1800. My wife and I both graduated from McQueen and have been in Northwest Reno most of our lives. I have more on my mind but will conclude for now. Thank you for taking the time to speak with us and read through some of my initial thoughts and concerns.

Sincerely,

TeJay Harvey
775-225-5531

Good evening,

My name is Tejay Harvey. My wife and I, along with our three children, live on the western boundary of the proposed development. When I initially looked at the proposed development plans left hanging on my door, my first thought was, "Why would they be trying to squish so many houses into such a small area?" Clearly the only answer can be profit. The proposed development does nothing to benefit the community around it, instead it will burden its neighbors with increased traffic, dangerous ingress egress conditions for those both on 7th and Rhode Island Drive, a loss of property values and by adding more children to an already overburdened neighborhood and school system. Not only that, Greater Light Christian Center is a great neighbor and pillar of the community, that exists to serve those around them. The city of Reno's Master Plan states that it values responsible and well-managed growth, this is not that.

My next question to myself after reading the handout provided, is what is a cluster development? I found the Reno Municipal Code and read that a cluster development is defined as a development "encouraged to support the protection of sensitive natural resources, viewsheds or other unique site features; promote fire safety within the wildland interface; provide opportunities for shared common open space; protect documented wildlife corridors; and provide a more gradual transition to the unincorporated county and public lands." The definition I just read has zero correlation to the 3.72 acres locked in the middle of an existing neighborhood, that we are discussing today. This alone should disqualify this entire development. The one and only reason to call this a cluster development is to skirt the zoning requirements of SF-8 housing and attempt to reduce the lot size requirements.

After finding the Reno Municipal code on cluster developments I continued to read and found more and more code violations within the proposed development.

To list a few: RMC 18.04.903 General Standards for Residential Districts Section 6.a.1. states one goal is to provide more open space. There is no open space designated for greater

community use, in this plan. The open space that is included for the subdivision, is partially designated as two retention ponds.

6.a.2 states the development will have no adverse impacts on adjacent properties. You will be building two story homes against my back fence, two per current property that will drop my property value by a minimum of \$30,000 for the loss of a city view alone. Then on top of that you will increase traffic, increase parking in surrounding neighborhoods and take already limited community resources, such as adding students to the current title one school Grace Warner that these students will be zoned for.

Section 6.b.1. states that cluster developments may be eligible for exceptions to minimum lot standards. Section 6.b.2. states modification to lot sizes may only increase the density of the development by 15%. SF-8 zoning states interior lots must be no less than 6000 sq foot (most in our neighborhood are more) this development is proposing the average lot to be only 3500 sq foot. That is an increased density of 42%, far exceeding the 15% limit defined in a cluster development.

Section 7.d. Single Family Residential Adjacency Standards 1. Parcel Size Matching states, "The minimum lot sizes identified in the land use designation of the immediately adjacent property shall be maintained at the edge of the proposed subdivision. This means putting two properties per one existing property, as proposed, is not allowed.

Section 7d.2. Buffering, does provide one exception to the adjacency standard above; however, it states that if the buffer zone were to remain natural vegetation it must be 100 feet or $\frac{1}{2}$ of the average minimum lot depth, for us that would be about 60 feet. Or, if fully landscaped it must be 30 feet wide. Looking at the proposed plan, the buffer zone is mostly 17 feet until it gets to the last property where it bottle-necks and is only five feet from the back of the existing neighborhood boundary to the proposed boundary. This is clearly not in compliance with the code.

Section 7.d.3 Yard matching further states that, "rear yard widths of the proposed development shall match the rear yard widths of the existing development." The proposed rear yard widths of this development are approximately half of the existing development. Again, clearly against code.

The development plan addresses the buffer zone on page 11 claiming they are providing a minimum seventeen foot (17') landscaped buffer which will be located between the existing residents to the west and the proposed lots, resulting in thirty-seven feet (37') of separation from the property line to the west and the proposed houses.

They are trying to include the 20 feet of backyard within the homeowner's property, from the fence until it touches the house, as the 37 feet of separation. So, existing neighborhood fence, plus 17 feet of buffer, then fenced back yard, and another 20 foot to the home. RMC code clearly implies the buffer is between property lines and not structures. There are pictures in the code that can help the developer understand if necessary. Additionally, the Stampede plan drawings show house 11 as only having a 5-foot set back not 17 feet.

Ultimately though, all the discussion on reduction of lot sizes, buffers, and yard matching is irrelevant because as I stated in the beginning, there are zero reasons to call the proposed development a cluster development. Any development should meet all SF-8 zoning requirements period. Any deviation from SF-8 is simply to attempt to skirt zoning laws without actually having to change the zoning.

Lastly, I would like to share that about one year ago a drunk driver drove up onto the grass and destroyed the GLCC church sign. If the homes in this proposed development were there, that car would have either been in a bedroom or the living room of one of those houses. The houses are only set 10 feet back from the roadway. 7th street is a dangerous high traffic thoroughfare that has been plagued from top to bottom with accidents. A little over ten years ago a passenger was impaled on a fence post on the corner of Heights and 7th, about 7 months ago an RTC bus went through a neighbor's fence. There was an article a little over a year ago, about a home that had been hit four times since the owner moved in. The development plan states there is no need for a traffic study because the peak travel will be minimal; however,

developers do not know the history of traffic accidents on seventh nor take into account the incline/decline of the street at the proposed intersection as well as the curve.

Thank you for taking the time to listen to my concerns.

TeJay Harvey.

Comments regarding Case Number LDC25-00003

2400 W 7th Street

- A.** The Planning Commission cannot reasonably find that the proposed development is compliant with the Reno Municipal code defining a Cluster Development and therefore should not proceed. Moreover, within the Cluster Development provision, the developer is picking and choosing to follow codes that benefit them and ignoring simple, straight forward RMC codes that don't benefit them. RMC code is law, this development cannot move forward as is, and be compliant with the law. (Chapter 18.04 Article 9, Cluster Development, pasted below)

Regarding the Cluster Development designation defined below and relating to 2400 W 7th:

There are no sensitive natural resources, viewsheds, or other unique site features on this site. There is no wildland interface, no wildlife corridors, and no transition to the unincorporated county and public lands. The only remaining factor that could justify development under the cluster development provision is if this site can provide opportunities for shared common open space. Open Space as defined and intended by the Cluster Development provision, is clearly not being met nor is there even an honest attempt at meeting the definition of Open Space.

Evaluating the "open space" at 2400 W 7th

1. The developer states there is .86 acres designated as open space. This is not factual. Using the numbers provided by the Tentative Preliminary Block Plan in the Stampede Plan; excluding the 28 lots, public roadway and private drives, the remaining land is .69 acres.
2. The remaining .69 acres includes the five-foot set-back that runs the eastern and southern property boundaries. It also includes the 17foot designated buffer of the western boundary. These buffers and setbacks are required by code and can absolutely not be considered open space, especially the southern boundary. Open space is defined as areas that are public and intended to be used by the community. No one wants children playing or people walking between the canal and the fence line of these properties. The developer is not intending this either as there is no trail system included.
3. Excluding the setbacks and buffer (mentioned above), there remains three areas of potential open space. These areas total approximately 16,000 square feet, a total of less than 10% of the 3.72 acres. Of the three remaining areas, per the preliminary grading plan, two of the areas are designated as retention ponds.

In summary, when looking over the proposed plans for 2400 W 7th and reviewing the points above, no one would reasonably conclude there is any intention of preserving open space in this development. The Planning Commission has a duty to find that this development does not intend to use the cluster development provision for its intended purpose but ultimately wants to designate it as cluster development to reduce lot size. The only reason there is any left-over space at all is due to the awkward shape of the parcel. Cluster developments are intended to

“cluster” development away from a hillside, a rock outcropping or other site features, or to preserve a substantial amount of open space, creating a usable area “providing opportunities for shared common open space” and “providing more open space”. Nothing about this proposed development is intending to preserve any sizable amount of open space nor create any functionable area for shared use. There are no site features being preserved, the only viewshed in the neighborhood will be blocked with 28–35-foot homes, there is no open space left to utilize. This is not a Cluster Development; it is an attempt to infill as many homes as possible onto this 3.72-acre plot. Lastly, the Cluster Development should have no adverse impact on adjacent properties. This development does not match the surrounding neighborhood. The current neighborhood exists primarily of one-story homes or homes with a daylight basement. The surrounding neighborhood includes seven to ten thousand square foot lot sizes on average. The proposed development will negatively affect surrounding home value as these homes are crammed into a fraction of the footprint of all surrounding homes and simply do not match. Additionally, proposed homes in this development will block the mountain and city views of the properties on the western boundary negatively affecting their home values. If the development were to be one story homes, or even homes properly spaced apart homes as outlined in SF-8 zoning, views would still be available at least between homes or over roofs. Protecting viewsheds is a component of Cluster Developments. The development should not move forward under the guise of open space. The proposed development should conform to all SF8 zoning requirements. The planning commission has a duty to uphold the law, Reno Municipal code, and find against this development as proposed. If this development was reduced by 30 percent, the in-fill goals of the city would still be accomplished, and profit would still be made by a developer. Let’s move forward with a responsible development plan.

B. There are two more important points where this development is not in compliance with code 18.04.903 (a) 7. d. Single-Family Adjacency Standards. (Pasted Below)

1. Parcel Size Matching
2. Yard Matching

Parcel Size Matching states, “The minimum lot sizes identified in the land use designation of the immediately adjacent property shall be maintained at the edge of the proposed subdivision as depicted in Figure 4-16”

There is a caveat, if a “buffer zone” is established you may reduce parcel sizes. The buffer must be 30 feet of developed property.

The Western boundary, shown in the Preliminary Lot and Block Plan, shows three different buffer widths. 23 feet, 17 feet then all the way down to 5 feet, between the property line of lot 11 and the neighboring development fence. The developer stated at the N.A.B they have a 37-foot buffer. They are measuring from the neighboring development fence line all the way to the physical home being built not the property line. Code shows that it is from the property line of the existing development to the property line of the proposed development, not structure to structure. The developer must draw new plans for the entire Western boundary creating a 30-foot buffer. This code is black and white there is no way around it.

Yard Matching states “The rear yard widths of the proposed development shall match the rear yard widths of the existing development as depicted in Figure 4-18.”

There is no caveat for this code and buffering does not appear to apply. As the code reads the widths of the existing subdivision and proposed subdivision must match. The proposed subdivision rear yards are approximately half the size of the existing subdivision.

The commission has a duty to ensure all development codes are being met. Codes are designed to protect neighborhoods and communities, please protect our neighborhood by developing under the proper SF-8 zoning requirements.

For reference:

RMC Chapter 18.04 Article 9

(6)

Cluster Development Cluster development is encouraged to support the protection of sensitive natural resources, viewsheds, or other unique site features; promote fire safety within the wildland interface; provide opportunities for shared common open space; protect documented wildlife corridors; and provide a more gradual transition to the unincorporated county and public lands.

a.

When Allowed Minimum lot size may be reduced through clustering of development if the applicable decision-making body finds that:

1.

The clustering proposal, compared with a more traditional site development plan, better attains the policies and objectives of this article, such as providing more open space, preserving existing trees and vegetation coverage, preserving view corridors, and preserving sensitive environmental areas such as stream corridors, slide areas, wetlands, and steep slopes;

2.

The clustering proposal will have no significant adverse impact on adjacent properties or development, or the applicant has agreed to adopt appropriate mitigation measures such as edge matching, landscaping, screening, illumination standards, and other design features to buffer and protect adjacent properties from the proposed clustered development; and

3.

The clustering proposal meets all other applicable requirements set forth in this article or in other applicable ordinances or regulations.

Chapter 18.04 Article 9 continued

d.

Single-Family Residential Adjacency Standards To provide adequate transition between varying sizes of single-family residential parcels designated for greater than one unit per acre density, one of the following methods shall be utilized:

1.

Parcel Size Matching The minimum lot sizes identified in the land use designation of the immediately adjacent property shall be maintained at the edge of the proposed subdivision as depicted in Figure 4-16; or

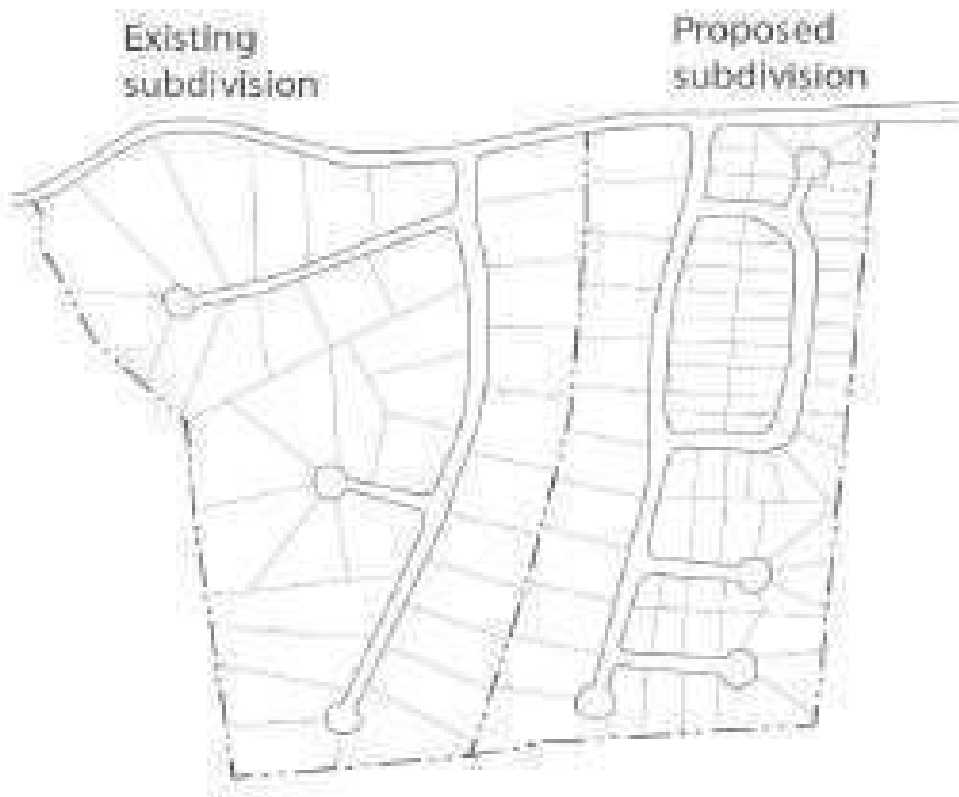


Figure 4-16: Parcel Size Matching

2.

Buffering A "buffer zone" shall be established. When the buffer remains natural vegetation, the buffer zone shall be equivalent to 100 feet or $\frac{1}{2}$ of the average minimum lot depth of the adjoining developed property, whichever is greater (see Figure 4-17). The buffer zone may be common open space for the proposed subdivision and may include paths, trails, or other subdivision amenities. The buffer zone shall be a minimum of 30 feet wide when fully landscaped and maintained. An equivalent combination of natural and landscaped buffer area may be approved by the Administrator; or

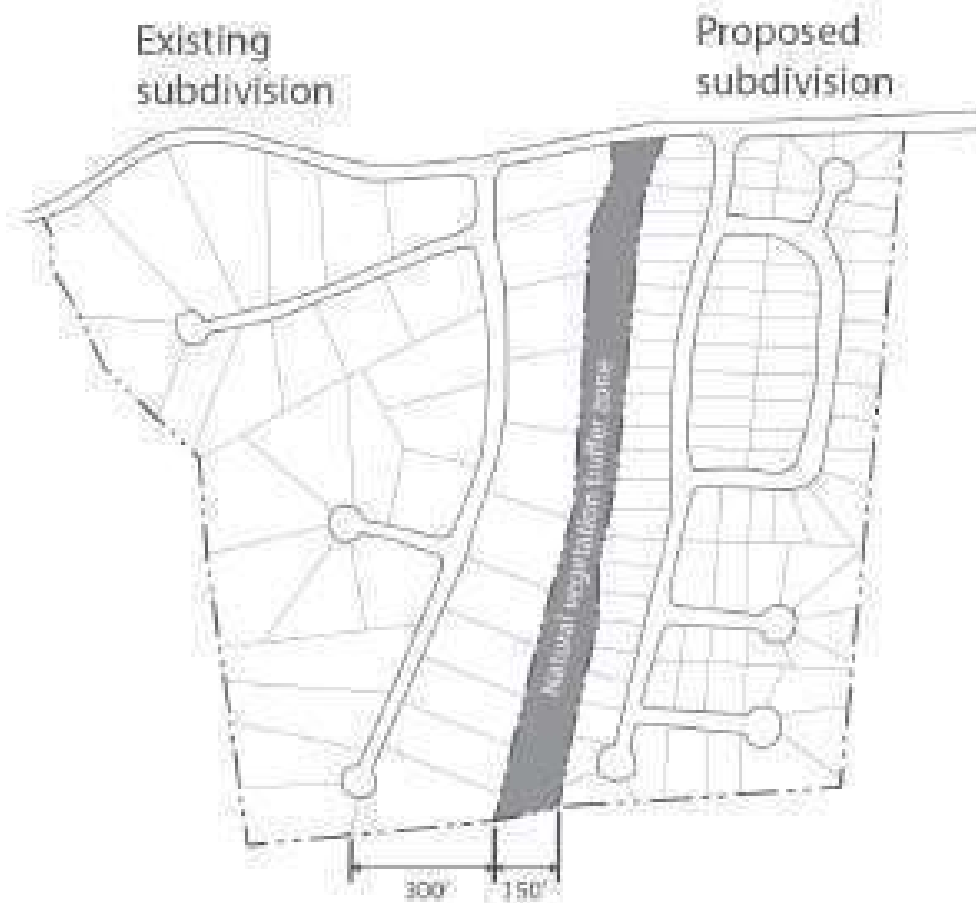


Figure 4-17: Natural Vegetation Buffer Zone

3.

Yard Matching The rear yard widths of the proposed development shall match the rear yard widths of the existing development as depicted in Figure 4-18.



Figure 4-18: Yard Matching

e.

Height Matching Lots proposed within a new subdivision that share a common property line with an established subdivision shall not contain structures within 100 feet of the shared property line that exceed the maximum height of the adjacent equivalent zoning district or land use district.

Good afternoon, Associate Planner Picotti and the Reno Planning Commission,

These comments are opposed to LDC25-00003, 2400W 7th Street, and are in reference to Wood Rodgers "Response to the City of Reno Initial Comments", included with the most recent submitted plans as of 9/18/2024. The commission requested Wood Rodgers (point 8) to justify the use of a cluster development, they did not answer nor justify how this parcel meets the criteria of a cluster development, instead they replied with a fanciful tale of how other potential plans for the parcel could be worse. We are not reviewing what ifs but are supposed to be reviewing why this proposed development should move forward as a cluster development.

The developer is requesting approval to build a cluster development at 2400 W 7th St., reducing lot sizes by nearly 50% when compared to traditional SF-8 zoning. I and other neighbors are opposed and alarmed that the city may consider putting a new development behind our homes that is completely out of characteristic with the surrounding neighborhood. As a result, I have received a crash course in reading and interpreting code as well as have talked to a few developers in the area regarding cluster developments; what they are, and how they should be used. The learning process has come with some misunderstanding of code relating to cluster developments as well as some aha moments Truth be told; it appears as though the engineering firm Wood Rodgers has also had some of these moments. Ultimately, I have learned how, when, and why cluster developments should be used; and this property in no way meets the definition of how, when or why cluster development should be used

One point I have been clear on since the beginning is that this plot of land does not fall under the criteria of a cluster development. The planning commission comments (number 8) asked Wood Rodgers to justify the use of a cluster development, and although they use a lot of words in their response, it is clear they did not, and cannot, justify the use of a cluster development. Their response states they potentially could be asking for a conditional use permit, and if approved, that development could have less of a setback and may be more intrusive to the neighborhood. They go on to say they are including a buffer (although they claim it is not required) and that if they developed townhomes no buffer would be required (not true). They summarize their response stating that by shrinking the buildable area the houses will be less wide and allow for views in-between houses. That is a silly argument and in no way speaks to justification as a cluster development. The potential view between homes is only 10 feet wide (a basketball hoop) and they are laid out in a way that even if you could see through the first row of homes the second set of homes are at a different angle and would block views. Wood Rodgers has previously pointed out they are not reducing density they are reducing lot size. This will increase building mass, not reduce it. SF-8 zoning is a misnomer in that mathematically you cannot fit 8 homes on an acre due to SF-8 lot size minimum requirements (6-7000 sq ft.), the zoning itself is inconsistent. Even if this property was a perfect square, you could not physically fit 28 homes on it. They could, however, use the same lay out with 19 homes, leaving more open space between homes and comply with code.

Ultimately, and most importantly, Wood Rodgers has not demonstrated that this development should move forward as a cluster development. In its most simple definition RMC Chapter 19.09 Article 4 All other terms defined, states, "Cluster Development– Moderate density attached or detached development, this is designated to protect sensitive areas to allow for common open space". Wood Rodgers did not state one justification as to how this is a sensitive

area or any other reason as to why this should fall under the definition of a cluster development. (If this were developed within SF-8 code it would have more open space, not less.) Instead, Wood Rodgers chose to try to justify their position through what ifs and it could be worse type arguments. We are not reviewing what Wood Rodgers may or may not be allowed to do, this is irrelevant to the justification requested by the planning commission. We are here to review what they have submitted, at this address, in this neighborhood, in this zoning district and within this specific plot of land. It should be clear to anyone who reads the definition of a cluster development or the code that defines it, that this is not a cluster development.

Thank you again for considering my comments and concerns.

TeJay Harvey

Leah Piccotti

From: T.M.H. <pic073mm@yahoo.com>
Sent: Friday, September 27, 2024 11:20 AM
To: Leah Piccotti
Subject: Fw: 2400 W 7th Street Tentative Map (LDC25-00003)

Hi Leah, I have a few quick comments on the document *Justification on Meeting the Definition of Cluster Development*.

2400 W 7th Street Tentative Map (LDC25-00003)

Cluster Development-

Moderate density attached or detached development, that is designed to protect sensitive areas to allow for common open space. Pg. 9-36-

1. Not a sensitive area.

2. Viewsheds are not protected. Utilizing buffers does not protect viewsheds.

Initial grade plan showed a 6-foot elevation difference from 2605 to the eastern boundary. This document says there is a 9-foot elevation difference. I believe that would put home 14 below the grade of the Highland ditch. Don't think that's possible.

Even if this grade is accurate, (figure 2) homes A, B, D and E would have to stand on their roofs to see over adjacent homes. This is not protecting views.

3. Common open space- No usable identified common areas. Under unique site features, Eric says a drainage channel will run along the western boundary; and "The proposed retention facilities accommodate for the unique site features while supporting protection of the viewshed but limits the opportunity for usable common areas." He then says, "The common area will also provide onsite retention ponds and drainage channels for capturing and conveying onsite, as well as offsite flows." Which is it? The drainage and retention areas limit the usable common areas, or the common areas include drainage and retention. The only identified common area in this document is in the southwest corner. The access to this area is a five-foot-wide gap between the neighbor's fence and property line of home 11. I would not believe this would be up to code for a walkway. Also, this seems like it would enable nefarious activities since this area would be tucked away and out of view. I imagine homeless and teens smoking or other, would like this tucked away area.

4. Drainage- "the developed site is not permitted to continue to drain to the Highland Canal". Current property does not drain into canal. No erosion/runoff lines exist to support this statement. During times of heavy rain or winter, the field gets wet and absorbs runoff. It is flat all around the canal, runoff does not reach it. With a landscaped and paved development, the water will not absorb but careen into the ditch, hence the need to retain the water.

5. Section 6 states mitigation measures are required. Section 6 Cluster development letter (a) 2. “mitigation measures such as edge matching, landscaping, screening, illumination standards, and other design features to buffer and protect adjacent properties from the proposed clustered development; and the clustering proposal meets all other applicable requirements set forth in this article or in other applicable ordinances or regulations.”

I left a message with you a few days ago after talking to the neighbors. Tim and Seanna Smith, 2575 Everett, planned to write you an individual message as to what they may support, but were on a similar page with the rest of the neighbors. Neighbors seem to be in agreement that any development should have minimum lot sizes as defined by SF-8 and be a responsible plan that is consistent with current neighborhood. Some neighbors said they would only support single story homes while others may be okay with a mixture. I am always open to chat if you would prefer.

Thank you for your effort and consideration,

TeJay Harvey

775-225-5531

TeJay Harvey

Caseworker I

Northern Nevada Transitional Housing

775-977-5905

NNTH (Main) 775-977-5900

Leah Piccotti

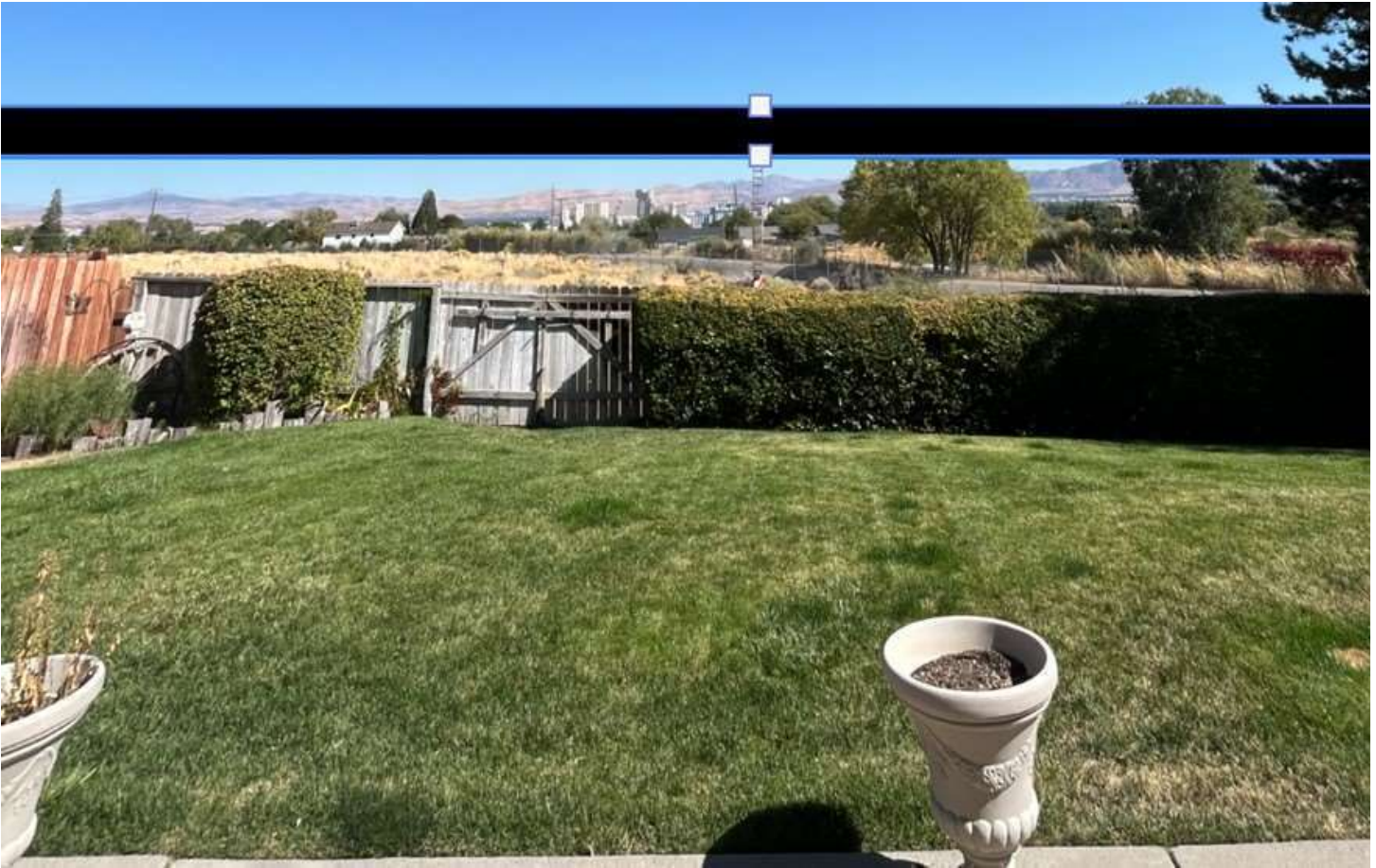
From: T.M.H. <pic073mm@yahoo.com>
Sent: Wednesday, October 2, 2024 1:41 PM
To: Leah Piccotti
Subject: Fw: 2400 W 7th LDC25-00003

Pictures of where rooflines will approximately fall. Homes on SE boundary do not allow views between houses due to 45 degree offset.

Wood Rodgers Elevation and Grading 2605 and 2615 Everett Dr



2615 view from back patio to estimated height of home 13 (point E). 28' roofline. From highland ditch grade/base elevation. (Home 11 will be much more obtrusive due to closer proximity.)

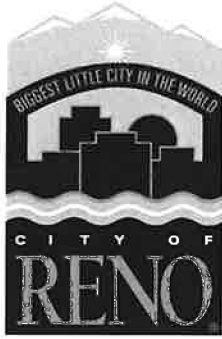


2605 Everett view from 2nd story deck to estimated height of home 14. 28' roofline. From highland ditch grade/base elevation.



Proposed homes will need to sit slightly higher than base grade level to allow for drainage away from home. Rooflines will sit slightly higher than pictured above.

TeJay Harvey
Caseworker I
Northern Nevada Transitional Housing
775-977-5905
NNTH (Main) 775-977-5900



Reno Neighborhood Advisory Board Attendance/Comment Request Form

FORM MUST BE FILLED OUT COMPLETELY

DATE: 8/13 AGENDA ITEM NO. C1

NAME: Tim Harney

ADDRESS: 2605 Everett

I REPRESENT: TeJay Harney

I AM IN ATTENDANCE CONCERNING : LDC25 (2400 W 7th)

DO YOU WISH TO MAKE A STATEMENT: YES: X NO:

IN FAVOR: IN OPPOSITION: X

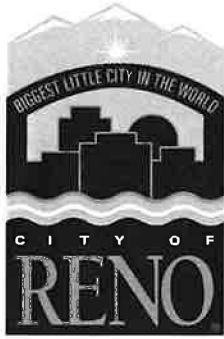
RENO RESIDENT YES: NO: X

NOTE: GENERAL POLICIES FOR ADDRESSING NEIGHBORHOOD ADVISORY BOARD

- *LIMIT COMMENTS TO THREE MINUTES OR LESS
- *15 MINUTES PER SIDE ON ITEMS WITH OPPOSITION
- *AVOID REPETITIVE REMARKS

**THE NEIGHBORHOOD ADVISORY CHAIR AND BOARD REQUEST
THAT ALL CONCERNS BE EXPRESSED IN A COURTEOUS MANNER**

THANK YOU FOR YOUR COOPERATION AND PARTICIPATION



Reno Neighborhood Advisory Board Attendance/Comment Request Form

FORM MUST BE FILLED OUT COMPLETELY

DATE: 8.13.24

AGENDA ITEM NO. C1

NAME: Teloy

ADDRESS: 2605 Everett Dr Reno NV 89503

I REPRESENT: Everett/neighbors

I AM IN ATTENDANCE CONCERNING : 2400 W 7th St

Item C

DO YOU WISH TO MAKE A STATEMENT: YES: X NO:

IN FAVOR: IN OPPOSITION: X

RENO RESIDENT YES: X NO:

NOTE: GENERAL POLICIES FOR ADDRESSING NEIGHBORHOOD ADVISORY BOARD

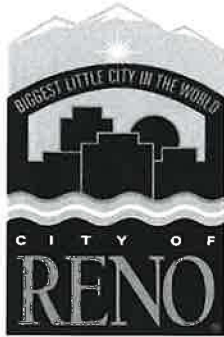
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*15 MINUTES PER SIDE ON ITEMS WITH OPPOSITION

*AVOID REPETITIVE REMARKS

**THE NEIGHBORHOOD ADVISORY CHAIR AND BOARD REQUEST
THAT ALL CONCERNS BE EXPRESSED IN A COURTEOUS MANNER**

THANK YOU FOR YOUR COOPERATION AND PARTICIPATION



Reno Neighborhood Advisory Board Attendance/Comment Request Form

FORM MUST BE FILLED OUT COMPLETELY

DATE: 8/13/24 AGENDA ITEM NO. C1

NAME: BARBARA KOROSA

ADDRESS: 2585 EVERETT DR. RENO, NV 89507,
lsierralady@att.net

I REPRESENT: _____

I AM IN ATTENDANCE CONCERNING : AGENDA ITEM C1

DO YOU WISH TO MAKE A STATEMENT: YES: X NO: _____

IN FAVOR: _____ IN OPPOSITION: X

RENO RESIDENT YES: X NO: _____

NOTE: GENERAL POLICIES FOR ADDRESSING NEIGHBORHOOD ADVISORY BOARD

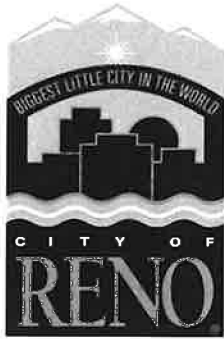
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*AVOID REPETITIVE REMARKS

**THE NEIGHBORHOOD ADVISORY CHAIR AND BOARD REQUEST
THAT ALL CONCERNS BE EXPRESSED IN A COURTEOUS MANNER**

THANK YOU FOR YOUR COOPERATION AND PARTICIPATION



Reno Neighborhood Advisory Board Attendance/Comment Request Form

FORM MUST BE FILLED OUT COMPLETELY

DATE: 8/13/24

AGENDA ITEM NO. C-1

NAME: Ashleigh Harvey

ADDRESS: 2605 Everett Dr
Reno, NV 89503

I REPRESENT: Everett/neighbors

I AM IN ATTENDANCE CONCERNING : Development

DO YOU WISH TO MAKE A STATEMENT: YES: ☒ NO: ☐

IN FAVOR: ☐ IN OPPOSITION: ☒

RENO RESIDENT YES: ☒ NO: ☐

NOTE: GENERAL POLICIES FOR ADDRESSING NEIGHBORHOOD ADVISORY BOARD

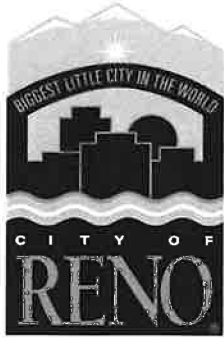
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*AVOID REPETITIVE REMARKS

**THE NEIGHBORHOOD ADVISORY CHAIR AND BOARD REQUEST
THAT ALL CONCERNS BE EXPRESSED IN A COURTEOUS MANNER**

THANK YOU FOR YOUR COOPERATION AND PARTICIPATION



Reno Neighborhood Advisory Board Attendance/Comment Request Form

FORM MUST BE FILLED OUT COMPLETELY

DATE: 8/13/24

AGENDA ITEM NO. C-1

NAME: Evad Harvey

ADDRESS: 2609 everett drive

I REPRESENT: Everett / neighbors

I AM IN ATTENDANCE CONCERNING : Development

DO YOU WISH TO MAKE A STATEMENT: YES: ☒ NO: ☐

IN FAVOR: ☐ IN OPPOSITION: ☒

RENO RESIDENT YES: ☒ NO: ☐

NOTE: GENERAL POLICIES FOR ADDRESSING NEIGHBORHOOD ADVISORY BOARD

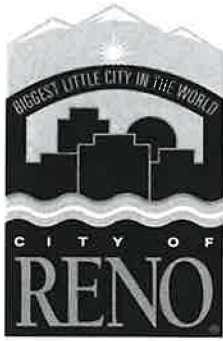
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**THE NEIGHBORHOOD ADVISORY CHAIR AND BOARD REQUEST
THAT ALL CONCERNS BE EXPRESSED IN A COURTEOUS MANNER**

THANK YOU FOR YOUR COOPERATION AND PARTICIPATION



Reno Neighborhood Advisory Board Attendance/Comment Request Form

FORM MUST BE FILLED OUT COMPLETELY

DATE: 8-12

AGENDA ITEM NO. CI

NAME: Tim Smith

ADDRESS: 2575 Everett Drive

I REPRESENT: Myself

I AM IN ATTENDANCE CONCERNING: CI

DO YOU WISH TO MAKE A STATEMENT: YES: X NO:

IN FAVOR: IN OPPOSITION: X

RENO RESIDENT YES: X NO:

NOTE: GENERAL POLICIES FOR ADDRESSING NEIGHBORHOOD ADVISORY BOARD

- *LIMIT COMMENTS TO THREE MINUTES OR LESS
- *15 MINUTES PER SIDE ON ITEMS WITH OPPOSITION
- *AVOID REPETITIVE REMARKS

**THE NEIGHBORHOOD ADVISORY CHAIR AND BOARD REQUEST
THAT ALL CONCERNS BE EXPRESSED IN A COURTEOUS MANNER**

THANK YOU FOR YOUR COOPERATION AND PARTICIPATION

Leah Piccotti

From: Eric Hasty <ehasty@WoodRodgers.com>
Sent: Thursday, September 12, 2024 1:35 PM
To: Leah Piccotti
Subject: 2400 West 7th Street Neighborhood Meeting
Attachments: Sign_In_Comments_N_Meeting_09102024.pdf; Neighborhood_Meeting_09102024.pdf; post card_meeting notice.pdf

Leah,

Attached are the comments received from the neighbors who attended the Applicant Hosted Neighborhood Meeting for 2400 West 7th Street TM and MSPR on September 10, 2024. Approximately 13 neighbors (20-25 people) attended the meeting. Most were adjacent neighbors to the west along Everett Drive. There were several concerns addressed, many of them captured in the comment cards. Overall, the main concerns we heard were their views being blocked by the proposed development and the standards outlined in RMC Section 18.04.903 (a)(7) specifically, the parcel size matching, buffering, or yard matching.

Our response was that these standards do not apply to this property since under the applicability section it states, “New land divisions adjacent to lower density residential zoning districts shall comply with the following standards, as applicable.” Since the adjacent properties to the west, north, east, and south are zoned Single-Family Residential 8 dwelling units per acre (SF-8) and the project site shares the same zoning designation of SF-8. This section does not apply; however, to meet the findings of the cluster development we are required to show that the cluster development better attains the policies and objectives of Article 9, and through the common area provided we are able to provide a buffer between the existing lots to the west and the proposed lots. Although the buffer does not have to meet the minimum 30-foot landscape buffer requirements, the project is able to provide a minimum 16-foot landscaped buffer. Which in combination with the rear setback, pushes the homes farther away from property line to the west when compared to the minimum 20-foot rear setback of a traditional SF-8 lot subdivision, and better meets the policies of Article 9. Furthermore, with the separation in elevation between the existing homes to the west and with the increase in setbacks between the common area buffer and proposed grading, the houses will set lower than the existing homes and help to preserve the view corridors. Finally, by reducing the lot size and thus shrinking the buildable area, the houses will be less wide than those of a traditional lot, reducing the building massing and allowing views between the homes when compared to a traditional single-family detached home in the SF-8 zoning.

These responses will be recorded in the response to planning and engineering comments that will be submitted to the city next week by September 18th but I wanted to give you an overview of the meeting.

Please let me know if you have any questions.

Thanks,

Eric Hasty, AICP | Project Planner

Wood Rodgers, Inc. | www.woodrogers.com |
775.823.4068 Main
775.823.9770 Direct

775.823.4066 Fax

ehasty@WoodRodgers.com

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2400 West 7th Street

**Tentative Map & Major Site Plan Review
LDC25-00003**

Neighborhood Meeting
September 10, 2024

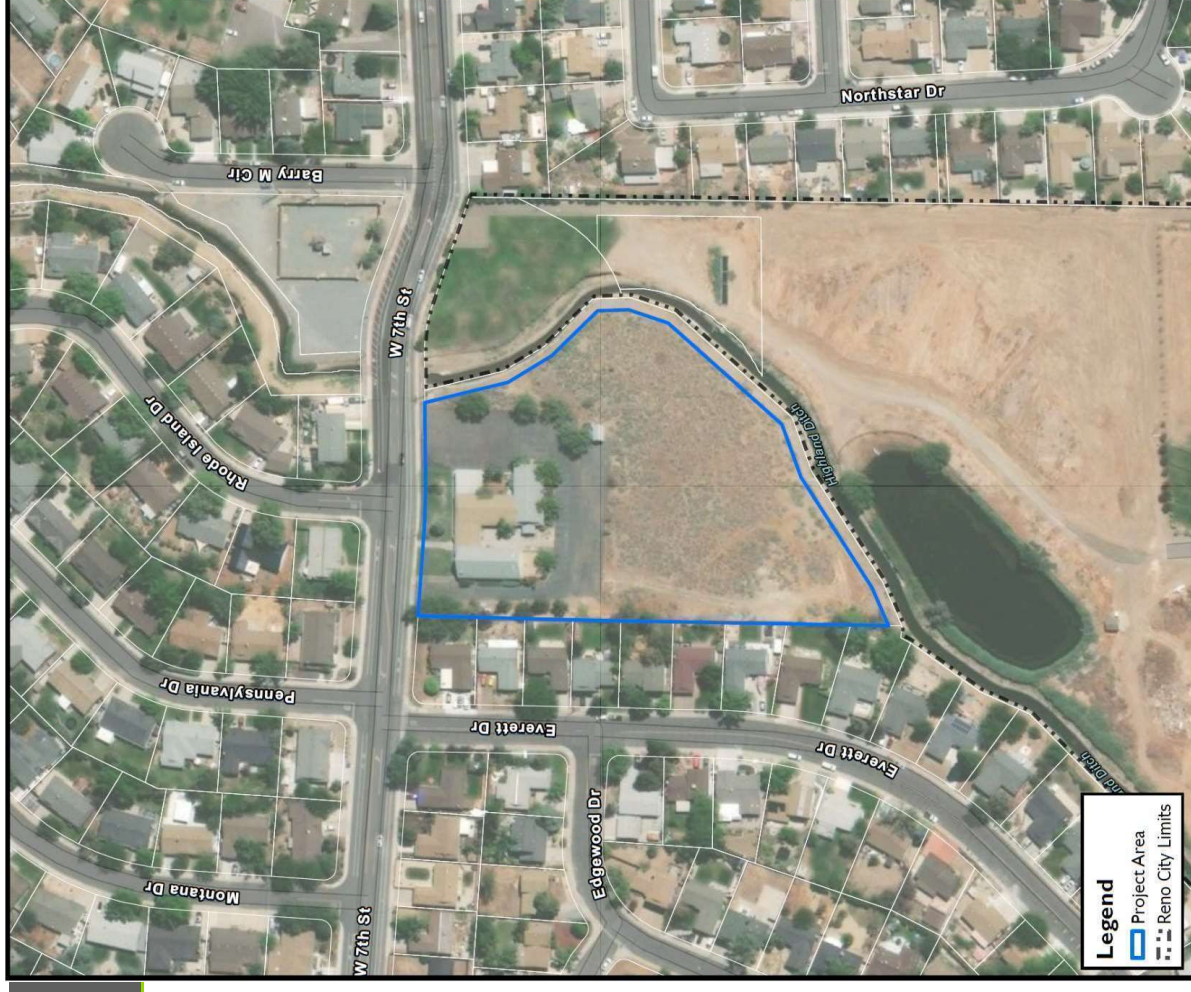


WOOD RODGERS
BUILDING RELATIONSHIPS ONE PROJECT AT A TIME

Location

City of Reno

- » 2400 West 7th Street
- » APN 005-122-09
- » **±3.72-acres**
- » Episcopal Diocese of Nevada (church)
- » **Infill** site currently served by City of Reno services and utilities



Proposed Request

Tentative Map & Major Site Plan Review:

- 28 lot subdivision
- Cluster development to allow reduced lot size
- ± 1.32 acres of common area (± 0.85 acres landscape)



Master Plan: Single-Family Residential (SF)

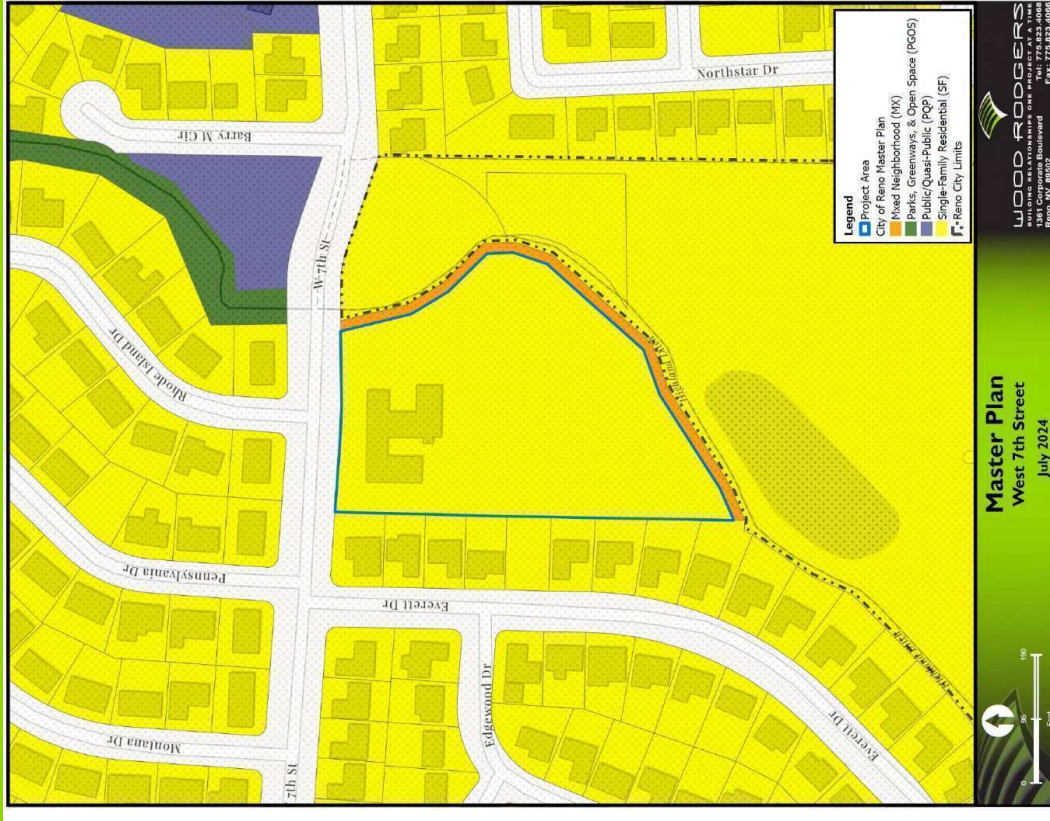
ReImagine Reno Master Plan: Infill /Redevelopment

» The Truckee Meadows Regional Plan and the City of Reno Master Plan both provide a robust framework for encouraging compact growth. The Master Plan **prioritizes infill development** and redevelopment in targeted areas, **generally located within the McCarran Boulevard.**

» Master Plan policies and the Land Use Plan **support the creation of more mixed density neighborhoods with a wider range of housing types**, either through the voluntary modification of existing PUDs or **through infill** and redevelopment of existing areas.



WOOD RODGERS

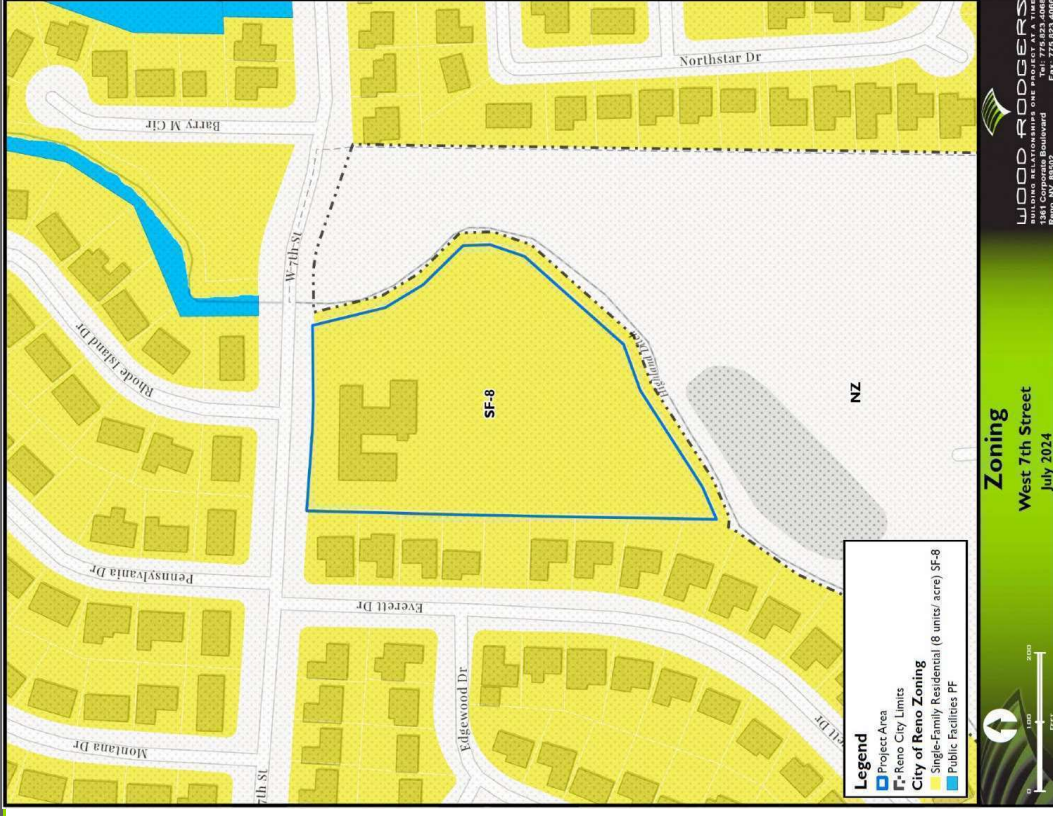


Master Plan
West 7th Street
July 2024



WOOD RODGERS
BUILDING TRANSFORMATIONS ONE PROJECT AT A TIME
1000 W. 7th Street, Suite 100
Reno, NV 89502
Phone: 775.853.4000
Fax: 775.853.4006

Zoning: Single-Family Residential 8 du/ac (SF8)



SF8 is a conforming zoning designation within the SF Master Plan

- » Allows a maximum of 8 dwelling units per acre (du/ac)
- » Total site is +3.72-acres
- » $3.72 \times 8 = 29.76$ (29 homes/units/lots)
- » **Proposing 28 lots**
- » Proposed density is **UNDER** the allowed maximum density. Not asking for an increase in density.

SF8 Zoning Allowed Uses

SF8 Zoning Uses with Conditional Use Permit (C):

- ✓ Duplex
- ✓ Triplex
- ✓ Fourplex
- ✓ Multi-Family (single lot duplex, apartments, etc.)

SF8 Zoning Uses Permitted (P):

- ✓ **Single-Family Attached (townhomes/condos)**
 - Does not require a cluster development
 - Setback only apply to perimeter of the site
- ✓ **Single-Family Detached**
- ✓ **Manufactured Home**
- ✓ **Mobile Home Subdivision**

[illegible]

SF8 Zoning: Design Standards vs Proposed

- Proposed Standards:**
- Density: 7.5 du/acre
 - Lot Size, min: 3,000 sf
 - Lot Width, min: 35 ft
- Proposed Setbacks, Minimum**
- Front = 10 ft
 - Side = 5 ft
 - Rear = 20 ft
 - Street-facing garage = 20 ft

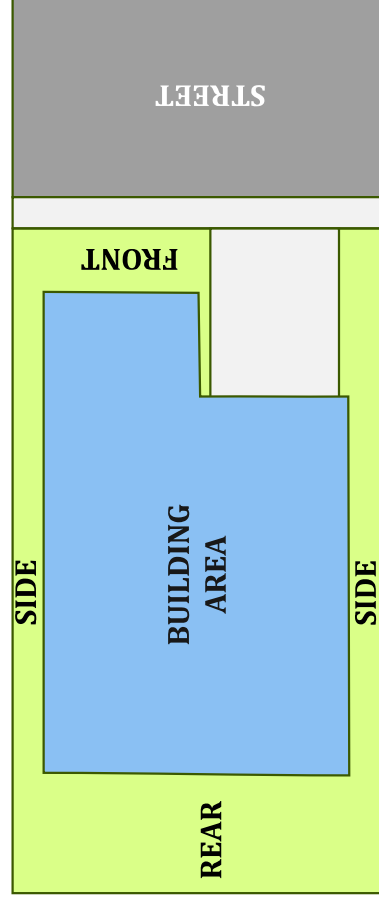
Table 2-9 SF-8: Lot and Building Standards			
	Lots	Corner Lot	Interior Lot
	Base density, maximum	8 du/acre	
A	Lot size, minimum	7,000 sf	6,000 sf
B	Lot width, minimum	70 ft	60 ft
	Setbacks, minimum [1]	Principal Building	Accessory Structure
C	Front	10 ft	[2]
D	Side	5 ft	5 ft
E	Rear	20 ft	20 ft/5 ft [3]
F	Street-facing garage	20 ft measured from sidewalk or planned sidewalk to face of garage	
	Height, maximum	Principal Building	Accessory Structure
	Height	35 ft	29 ft/16 ft/12 ft [4]
	Stories	2-5	[4]

Traditional SF8 Lot Configuration **vs** Cluster Development

Traditional Single-Family Development:

Minimum Lot Standards

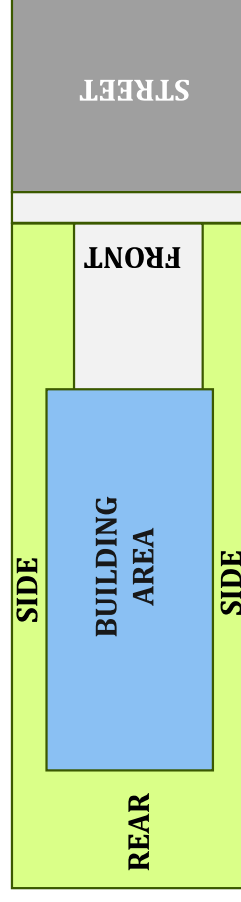
- 6,000 sq ft lot
- 60 ft lot width
- Front Setback: 10 ft
- Garage Setback 20 ft
- Side Setback: 5 ft
- Rear Setback: 20 ft



Proposed Cluster Development:

Minimum Lot Standards

- **3,000 sq ft lot**
- **35 ft lot width**
- Front Setback: 10 ft
- Garage Setback 20 ft
- Side Setback: 5 ft
- Rear Setback: 20 ft

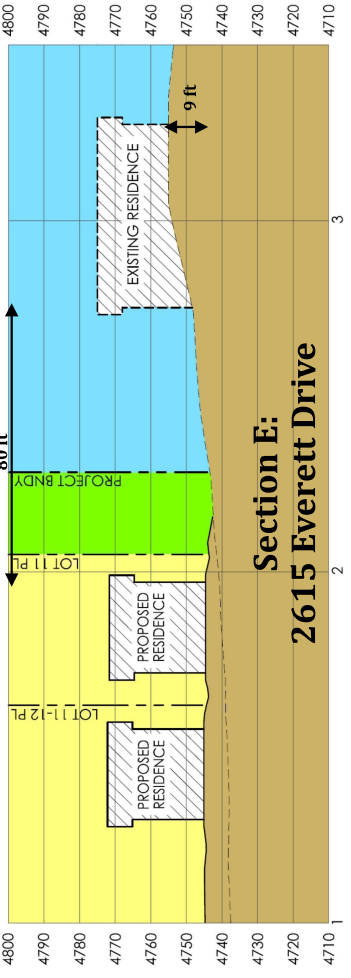
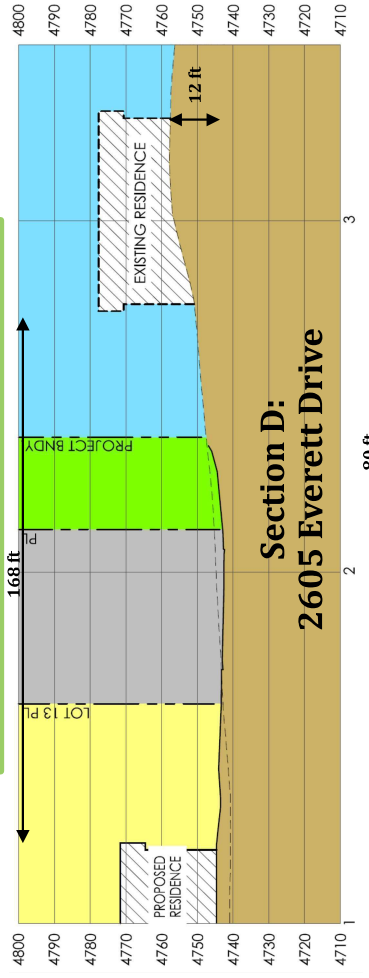
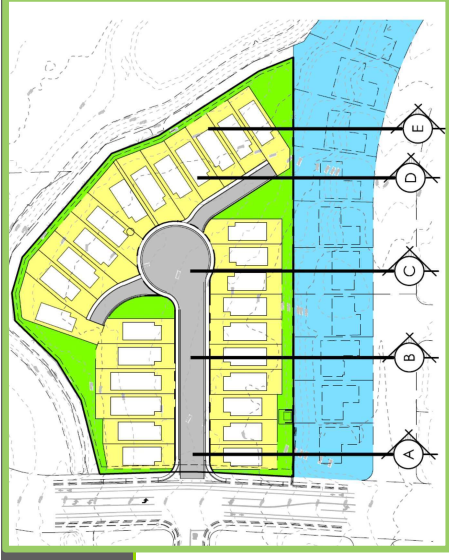
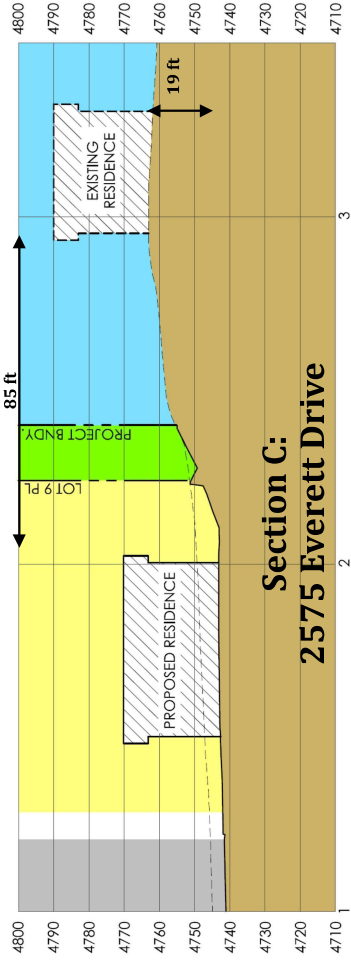
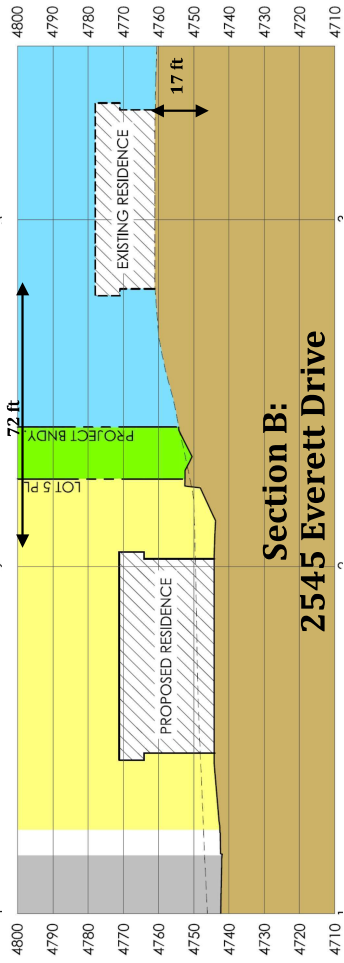
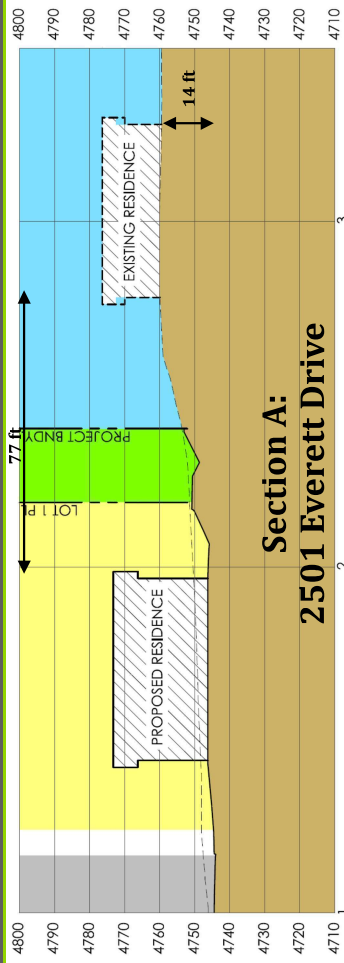


Cluster Development

- ✓ Provides ±0.85 acres of common area (23% of the site)
- ✓ Provides additional screening with landscaped common area
- ✓ Separation between backyards
 - Min. 22 ft on lots 1-3
 - Min. 16 ft on lots 4-10
 - Min. 5 ft on lot 11
- ✓ Allows for a greater distance between property line & homes
 - Min. 43 ft on lots 1-3
 - Min. 37 ft on lots 4-10
 - Min. 28 ft on lot 11
- ✓ Provides onsite retention, (no stormdrain facilities in W. 7th Street)



Elevation & Grading



Project Details

On-site Circulation:

- Single street aligning with Rhode Island Drive
- Two private driveways
- On-street parking
- Required parking = 56 spaces
- Parking provided = 119 spaces
- 5' sidewalk on both sides of street

Traffic Study:

- Anticipates 27 AM & 32 PM peak hour trips
- Existing church generates 158 peak hour trips during Sunday



Project Details

Model Options:

- Multiple elevations and floor plans
- Range from 1,851 to 2,395 square feet
- 3-5 bedrooms
- Minimum two car garage with driveway



Next Steps

- ✓ Ward 5 NAB Meeting (August 13th)
- ✓ Neighborhood Meeting (Sep. 10th)
- ❑ Compile comments from neighborhood meeting, provide any changes to City of Reno staff
- ❑ City of Reno staff report due (end of September)
- ❑ PC Meeting October 16th (tentative)

Eric Hasty

ehasty@woodrogers.com

775.823.9770



Additional Comments or Questions:

I would like someone to add crosswalk lights to 7th & Wyoming crosswalk. We have almost been hit (w/ my service dog) + constantly people speed + do not stop

Thank you

Name/Phone # or E-mail:

Maeybeth ^(AD) (Apeil) Calmner@att.net

Comment Card



WOOD RODGERS

Additional Comments or Questions:

If by nature a cluster development is not supposed to impede neighborhood values, what happens if it does? We have had people say it could drop values up to \$100,000. If we get an appraisal now and after and it shows a clear loss, what then?

Also, other cluster developments in the area seem to have 30-50% of open space. This development does not. How does this meet criteria. The landscape hardly seems like its "open" to the community.

Name/Phone # or E-mail:

Ashleigh Harvey 715-225-4848 scrunchnose@gmail.com

Comment Card



WOOD RODGERS

We would love trees in the landscape as another buffer.
Especially in open spaces and between properties.

The neighbors would rather see beautiful trees instead of homes.

Thank you!

Name/Phone # or E-mail:

Ashleigh Harvey 775-225-4848 scrunchnose@gmail.com

Comment Card



WOOD RODGERS

Additional Comments or Questions:

Owner: 2615 Everett Dr.

- Concerned for future value of home and taking away the views & values
- Concerned for the safety of the area
If the house on the corner catches fire will a fire truck be able to get there.
- Only 5 ft between yard and my fence. could cause a car to run thru my property especially when roads are icy.
- Sewer line in backyard- how will there be access for the city.
- Density violation, code violations
- Traffic- dangerous entry & exit zoning laws.

Name/Phone # or E-mail:

Thea Diamond 420-7685 (775)

Comment Card



WOOD RODGERS

Name	Phone #	Address	Email
Newboth Burns	834-5588	610 Award Circle	Colmnoens@att.net
MAC ROSSI	408-410.3062		
Thea Diamond	775-420-7685	21615 Everett Dr. 1	thea.diamond2@gmail.com
Gabby Avenir	530-306-3739	26720 Edgewood Dr.	gabriellaavenir@hotmail.com
Chin Tsa;	775-8480558	2501 EVERETT DR.	chintsa1@msu.com
William Trudell	775-525-4017	2521 Everett Dr.	lionintheflow@hotmail.com
Cody & VERONICA McMINN	775-824-1840	2515 EVERETT DRIVE	ronniptzer@SPEGLOBAL.NET
Glenn & Judy Goza	775-747-2239	840 Rhode Island Dr	reno-boy@hotmail.com
Ashleigh Harvey	775-225-4848	2405 Everett Dr	scrunchnose@gmail.com
Leslie Burns	775-686-9927	2501 Edgewood Dr.	burns.leslie.m@gmail.com
Hallie Madole	775-762-3741	860 Rhode Island Drive	hmadole@tncc.edu
Tim Smith	530-900-310	2575 Everett Drive Reno 89503	tsmith@usmcco.biz
Seafanna Smith	530-900-1470	2575 Everett Dr. Reno NV 89503	seafanna.smith@gmail.com

Neighborhood Meeting

Join us on **September 10, 2024** at
Grace Warner Elementary School
3075 Heights Drive, Reno NV 89503

5:30 pm to 6:30 pm

You are invited to attend a neighborhood meeting to hear a presentation about the proposed project known as **2400 West 7th Street** located on APN 005-122-09, which is the current location of the Episcopal Diocese of Nevada. A request has been made for a Tentative Map and Major Site Plan Review on the entire ± 3.72 acre site to develop the property with a 28 lot single-family detached cluster subdivision with ± 0.85 acres of common area. The meeting will be located in the Multi-Purpose Room. Following the presentation, the applicant's representatives will be available for questions. Please feel free to share this notice with your neighbors if they are not located within the 750-foot noticing radius of the project.

If you are unable to attend but would like further information, please contact:

Eric Hasty | ehasty@woodroddgers.com | 775-823-9770

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Fax: 775.823.4066



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